



Mildura Rural City Council

## Etiwanda Report

for  
Residential Development Plan  
for  
Mildura Rural City Council

May 2010



CLIENTS | PEOPLE | PERFORMANCE

DAVID LOCK ASSOCIATES  
URBAN DESIGN AND TOWN PLANNING





## Contents

Section 1: Introduction

Section 2: Context of the Site

Section 3: The Onsite Issues and Characteristics

Section 4: Engineering Constraints

Section 5: Residential Development Plan

Section 6: The Concepts - Indicative Components of the RDP

Section 7: The Concepts - Design Guidelines

Appendix 1: Summary of Consultation Feedback

Appendix 2: Summary of Consultation Amendments

Appendix 3: Summary of Amendments in Response to Submissions made at Exhibition

Appendix 4: Revisions Table



Section I

Introduction



## Final report – Mildura Residential Development Plans

This report is one of four that outlines the Residential Development Plans (RDPs) for four areas in Mildura that are expected to be the focus of residential development in the city in the near future. These areas are known as Etiwanda, Irymple, Riverside and Nichols Point. The RDPs identify the characteristics of development that will achieve the optimum benefit for both the incoming residents and the wider Mildura community by ensuring the new neighbourhoods are at once seamlessly stitched into their surroundings yet with their own distinctive character and identity. The concept seeks to ensure the people who will live in these areas will enjoy a good quality of life with minimal demands on finite environmental resources. The RDP does this by identifying the design characteristics that ensure these people can enjoy environments that they can be proud of, that are attractive, stimulating, facilitate greater sustainability and foster social engagement. This has been achieved by engaging the local community, council, stakeholders and the consultants in a collaborative process to ensure design conclusions are firmly based on a broad understanding of all the issues.

The process by which this study has been undertaken has three principal stages; understanding the context of the site (section 2), understanding the on site issues and characteristics that affect the development of the site (section 3), understanding the engineering constraints (section 4) and then identifying a concept that achieves a high standard of design and can be demonstrated to address the issues identified above (section 5 -7).

The context (section 2) identifies the relationship between the sites and the important social and physical infrastructure around which a community can coalesce (schools, shops, the city, etc).

The issues (section 3) identifies those factors felt to be important by the various parties who attended the first round of consultation in February 2006. (see appendix 1) and includes community, consultants, council and stakeholder observations. It is important to note that solutions were not invited at this stage so we could concentrate on the underlying issues and ensure we had as good an understanding of those as possible before we started drawing conclusions. Consequently recommendations have not been recorded so “we want larger blocks” were not recorded at this stage as what was of interest is the underlying concern or hope eg “because we value our rural outlook”, which is helpful. This is important if we are to treat all the issues equally and ensure the design agenda is not skewed by a suggestion that may not be the best way of achieving the proponents underlying objective.

The engineering constraints (section 4) relates mainly to dealing with drainage, sewerage and road issues, and seeks to identify and “lock in” the built form implications of ensuring the site can be adequately serviced and does not impact inequitably on its surroundings.

The concepts (section 5-7) are in three parts; the first part describes indicative road and open space layouts and identify other environmental enhancement projects where appropriate. The second part identifies the key characteristics of the different types of roads and open spaces to ensure they can fulfil the objectives of the study. The third part suggests the aspects of house and garden design that should be controlled if each house lot is to fulfil its responsibility in achieving a more liveable environment.

## The Process

This document was initially prepared for the second workshop and has been varied from the intermediate document prepared following the feedback from the workshop. The process undertaken can broadly be described as;

### **Stage 1. Site Review and Research**

Existing plans and studies (workshop 1)

### **Stage 2. Initial Stakeholder Consultation**

This established the issues that surround the potential development of the sites. These relate to the various agencies that are involved in development, the relevant council officers and members of the local community (workshop 2)

### **Stage 3. Understanding the Issues of and developing a Vision**

This stage involved developing a concept, testing it against our understanding of the issues and confirming our understanding of the communities concerns through a workshop held 31st March - 1st April.

At this workshop the attendees were asked whether we have understood their concerns correctly and if we have addressed them to their satisfaction. If we have not we will need to either change our understanding or explain it better. The ideas were explored and if appropriate amended. After the workshop sessions a final draft concept was prepared and the changes documented in order to ensure a wide ownership of the final design. Whilst the process is committed to treating everyones contribution with equal respect, there is no commitment

to making a change just because a change is requested. However an exploration of the consideration given to each submission can be found in appendix 3.

### **Stage 4. Focus Group Workshops**

To further develop and test the concept.

### **Stage 5. Preperation of Draft Development Plans**

Outlining the concept, their key components and describing what they are trying to achieve.

### **Stage 6. Exhibition**

To facilitate final consultation of draft plan (This occurred between 28th of October and 27th of November.)

### **Stage 7. Finalise Plan**

To consider submissions made on exhibition and amend where appropriate. This document has been prepared at the end of stage 7 and contains the final RDP for Etiwanda.

### **Stage 8. Council Adoption**

## Neighbourhood principles

The detailed design of the new neighbourhoods is envisaged to reflect the neighbourhood principles outlined under Clause 12.05.2 and in Section 56 of the Planning Scheme, which can be summarised as:

- Compact walkable neighbourhoods where neighbourhood Centres support local services and facilities. Reduced car use is encouraged because public transport is easy to use and walking and cycling are promoted.
- Environmentally friendly development where lot layout and design supports more energy efficient dwellings.
- Diverse lot sizes and a range of lot types will be provided to better meet future community housing needs.
- Integrated water management that conserves our drinking water and locally manages the quality of urban run-off.
- Socially responsible development that is connected, overlooked and contribute to safety and perceptions of safety.

## Detailed Design Principles

These neighbourhood principles will be implemented through application of detailed design principles;

- Provide an attractive sense of arrival at each entrance to the site and to each character precinct, through the use of distinctive and attractive landscape features
- Emphasise decision points in the movement network such as intersections through the use of attractive and distinctive built form and landscape
- Provide a logical, safe and connected movement network for pedestrians and cyclists
- Ensure that all streets and parks are overlooked by houses, to enhance their safety for pedestrians and cyclists
- Avoid the creation of streets dominated by garages through the use of rear lanes (where possible) to provide vehicle access around important community open spaces
- Promote traffic speeds and behaviour appropriate to a residential environment through the design of local streets and appropriate traffic calming measures
- Use a distinctive combination of views, landscape and built form in each street to create a sense of place and aid legibility
- Ensure that the swales and overland flow paths are attractive spaces even when dry
- Align secondary streets and lots to maximise the potential for solar access.

## Key Features

In addition to the principles described above, the key features embodied within this RDP are;

### ***Responding to Existing Characteristics***

The unique 45 degree grid layout of Mildura provides a challenge and opportunity to respond creatively. This arrangement is sub-optimal for solar access which is an important consideration given it represents the most abundant source of “free” energy to manage comfort within the house. Making best use of this resource requires lots which can facilitate development which can have their longside facing north.

However, in this instance the shape of the area, existing lot boundaries within in and the course of the drainage lines suggest that lots may be better orientated in keeping with the existing alignment. To do otherwise would result in many odd shaped lots which may meet with market resistance.



The vision plans seek to ensure desire lines are catered for, encouraging walking and cycling a relatively more attractive way of getting around for shorter journeys. Routes are designed to link up key destinations and site assets with primary movement routes to provide for direct and logical movement through the site. The road alignment is designed to respect drainage lines and use the natural fall of the land to provide an ecologically sustainable stormwater management system. The roads and open spaces are designed to retain healthy trees and uses them as a generator of local identity. The layout will create variations in density and promote distinctive building and landscape character to reinforce the sense of the area responding to its surroundings. The concepts incorporate variations in density to respond to existing development beyond the site. They introduce new qualities in parts of the site with little intrinsic quality.

### ***Open space and movement networks***

In keeping with best practice water sensitive urban design (WSUD) and as reflected in SPPF clause 12.07.2 public open space provision is focused on the stormwater management areas and a road layout that emphasises their importance. Reducing the impact of stormwater by incorporating WSUD to protect and enhance natural water systems and integrate stormwater management into the landscape. The resulting public open space network will contribute significantly to the area's character and provide parks within a comfortable walking distance of anywhere in the study area, and promote walking and cycling. Secondary streets are aligned where possible to provide a direct visual link to these key features and ensure that swale drains can be used to filter stormwater run-off and mitigate peak flows. This alignment can also capitalise on their value as an attractive view termination. The design of secondary streets and adjoining built form and landscape will provide a clear expression of the street hierarchy, which will help make the neighbourhood more legible and less homogenous. In residential 1 zoned land where the smaller lots make getting good solar access issues more of a challenge, most streets will be generally orientated north-south to ensure the long side of most lots face north to facilitate good solar access. Intersections will be designed to avoid conflict and ensure there is no ambiguity as to priority.

### ***Neighbourhood focal point***

A development node around a village green containing play, recreation and social infrastructure around a distinctive and striking landscape feature, is located at the junctions of the primary movement networks and (where possible) the drainage line to:

- Create a neighbourhood focal point - Concentrate demand for public transport services and other amenities where they can best be met
- Reinforce the clarity of the overall urban structure
- Create a more visually interesting experience as one moves through the new neighbourhood
- Generate local identity on a walkable neighbourhood scale—reflecting the characteristic density gradients of regional city in a rural environment.

### ***Developing Character areas***

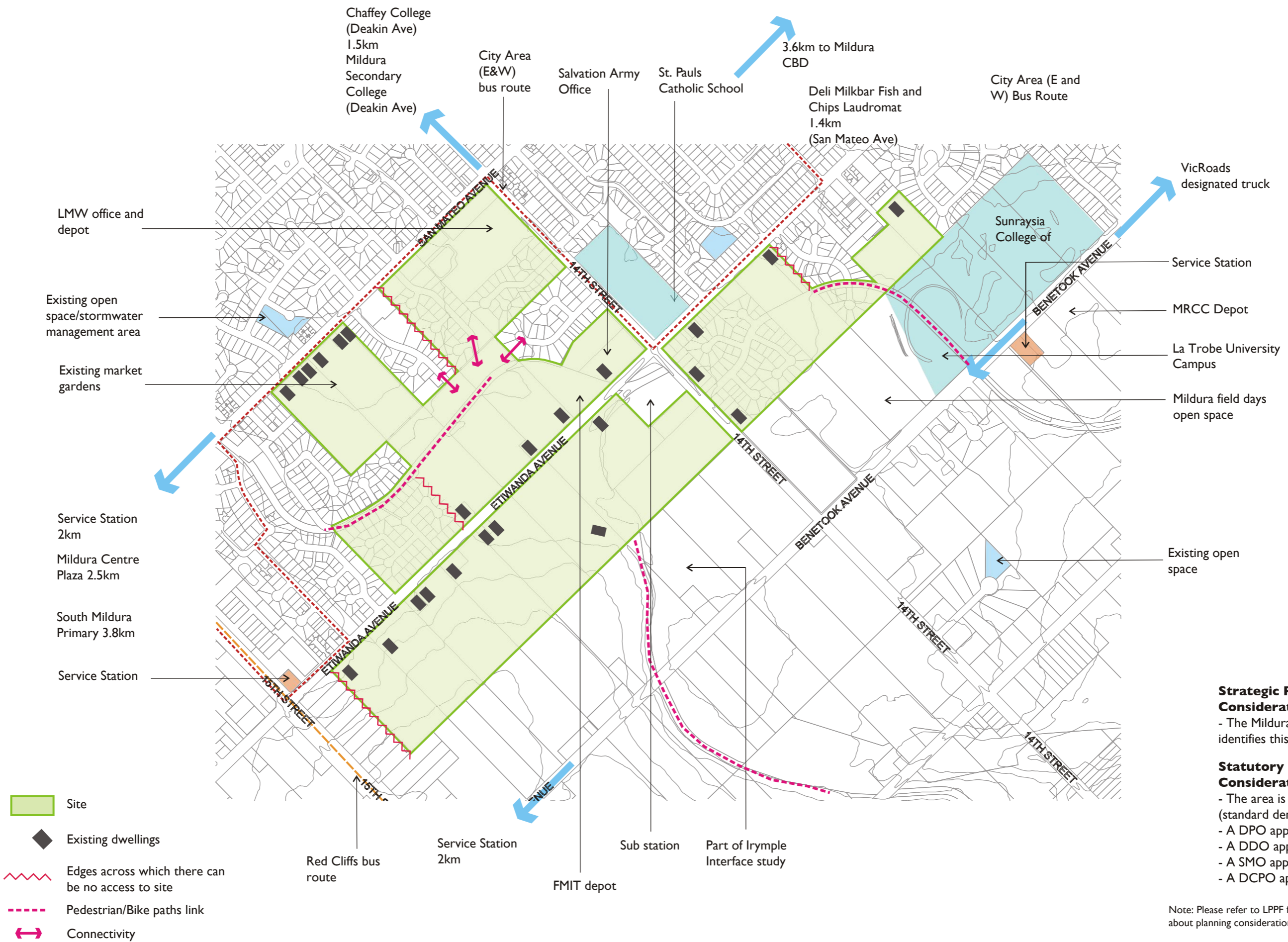
Landscaping, road layout, lot layout and lot size are all used to create a range of different character areas. This will enhance the visual interest and distinctiveness of the area and provide an appropriate way of accommodating the transition from urban to rural common to all areas. A green skyline and shaded streets will improve the amenity of the roads and other public spaces of the area and facilitate more pedestrian activity during summer.



Section 2

Context of the Site







Section 3

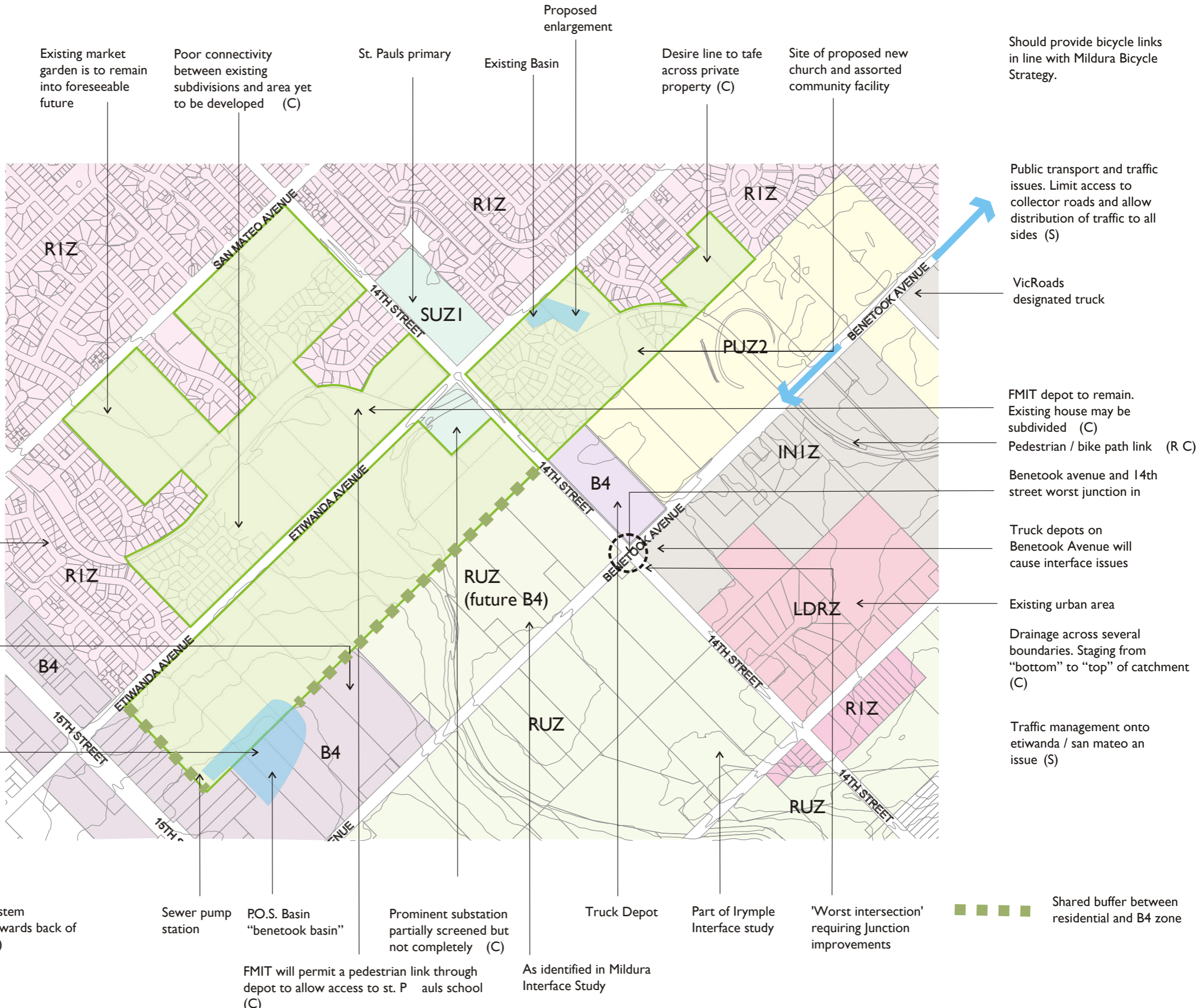
The Onsite Issues and Characteristics





**General Issues and observations**

- Residents keen to get started on the process (R)
- Fragmented area (C)
- Site crossed by former pipeline (RC)
- Lack of community focal point (S)
- Hot, dry, dusty environment
- Grid connectivity valued
- Grid not aligned North/South



Source:  
 (R) Resident  
 (C) Consultant  
 (S) Stakeholder

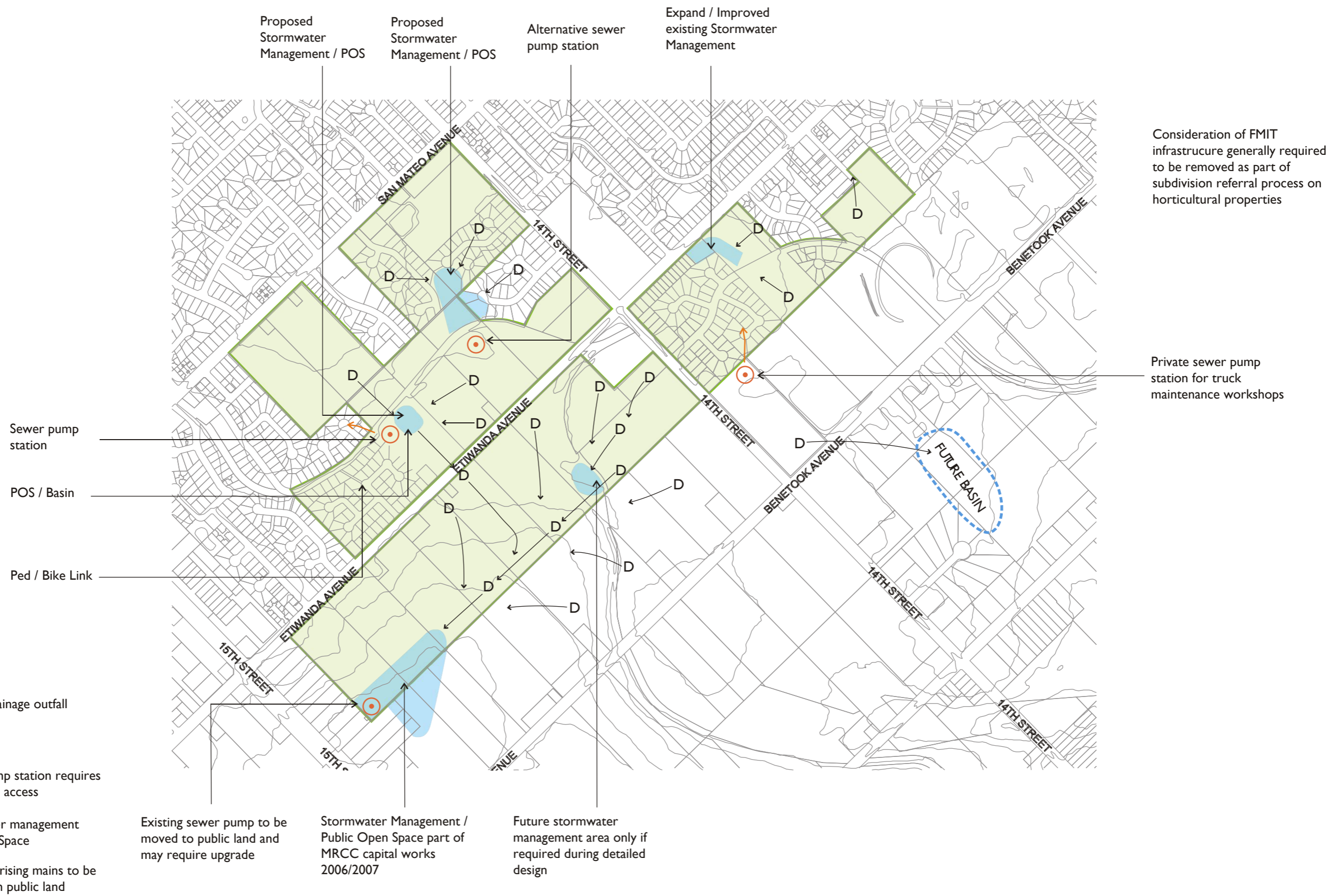




Section 4

Engineering Constraints





## RESIDENTIAL I ZONE

### DPO1

#### **Infrastructure & Servicing**

The site comprises the next staged development fronts for the “Etiwanda” area.

#### **Stormwater drainage**

- Mildura Rural City Councils (MRCC) is the relevant service authority for stormwater drainage and associated infrastructure.
- A brief assessment of existing topography and drainage services, and discussions with the Mildura Rural City Councils Engineering department have indicated that the drainage to these areas is possible via “Stormwater Management Areas”(SMA’s), within the determined catchment areas. The SMA’s will be able to connect to existing drainage infrastructure when specific Catchment Capital Works (CCW) improvements have been carried out.
- The SMA’s and CCW infrastructure works are included as a part of the existing and/or proposed Development Contributions Plans (DCP) for these areas. The DCP will provide specific information, including size requirements on the required contribution for each catchment area. If individual developments provide SMA’s or CCW works then due consideration/compensation will be assessed for their contribution to the catchments.
- Indicative drainage areas / catchments are included in the “Drainage and Wastewater” plans, in this report. The detailed design for developments will have to follow the intent of these plans, and should be formulated in conjunction with Councils Engineering department and any specified / required DCP works.
- Development of drainage concepts will have to be in accordance with current planning scheme controls and local MRCC policies.
- Water Sensitive Urban Design principles (WSUD) will have to be used, during development designs; in accordance with current planning scheme controls and local MRCC policies.
- Where appropriate SMA’S have been incorporated with public open space (POS) areas.

#### **Potable Water and Wastewater**

- Lower Murray Water (LMW) is the relevant service authority for potable water and wastewater. Consultation with LMW, during the design development stage, will clarify the specific infrastructure augmentation works and pump station requirement for wastewater connections.
- Water and wastewater services can be incorporated in developments by extensions to existing infrastructure. Some developments will require augmentation to existing systems.
- Designs will have to be carried out in accordance with current planning scheme controls and LMW policies.

#### **Electricity Supply**

- The provision of electricity infrastructure is not expected to cause any problems in these areas.
- Powercor will need to be consulted, during the design development stage, to confirm power supply easements; including transformer substation locations.

#### **Telecommunications Services**

- The provision of telecommunications services is not expected to cause any problems in these areas. • Telstra and Neighbourhood Cable will need to be consulted, during the design development stage, to confirm telecommunication service requirements, supply easements and alignments.

#### **Natural Gas service**

- The provision of natural gas service is not expected to cause any problems in these areas.
- Origin Energy need to be consulted, during the design development stage, to confirm natural gas service availability, supply requirements, easements and alignments.

#### **Horticultural irrigation and drainage**

The First Mildura Irrigation Trust (FMIT) currently provides horticultural water supply and drainage services to the area. The FMIT was consulted during the preparation of this plan. Their requirements are summarised below.

- The study areas were / are horticultural properties that may include irrigation and drainage services, within the allotments, controlled by the FMIT.
- Some of the existing services will become redundant; due to redevelopment to residential purposes. These services can be disconnected in accordance with the requirements of the service provider (currently FMIT). The developed areas will be excised out of the district as required by FMIT.
- Some of the existing services will have to remain; due to servicing areas outside the development plan area. These services can be identified in the design development stage, and redirected as required in accordance with the requirements of the service provider (currently FMIT). Typically service easements may be required over these services.
- Coordination and design of subdivision and/or developments should refer to the FMIT to determine specific service requirements and allow designs to accommodate services that are required to remain.

### ***Traffic & Public Transport***

- Subdivision and/or Development designs shall consider traffic and public transport design issues in accordance with current planning scheme controls, local MRCC policies and Vic Roads requirements.
- Mildura Bus lines were consulted, during this study, and did not indicate any issues with public transport to these areas. Designers shall liaise with the service provider to confirm any specific requirements.
- The Irymple area includes a railway reserve that cuts through part of the study area. Design of Subdivision and/or developments in these areas will have to coordinate with the relevant Railway Authority, if any works impact on the railway reserve, including but not limited to stormwater culverts.
- The Development Contribution Plan (DCP) considers requirements for road works, including intersections, bus stops/shelters and bicycle paths required due to the demand created in each area.

### ***Common Service trenching***

- MRCC has been using common service trenching designs in the area for many years.
- Common service trenching is encouraged with specific planning controls for all new developments and should be accommodated within road reserves and coordinated with all relevant service providers.

### ***Salinity Analysis and Management***

The details and implications of any salinity analysis done on the land should be considered in accordance with current planning scheme controls and local MRCC policies. Generally during the design phase a subdivision within the Salinity Management Overlay will require a Site Capability Report and Salinity Action Statement addressing the requirements of the 'Site Salinity Management Plan (Final Report) REM 2004'.



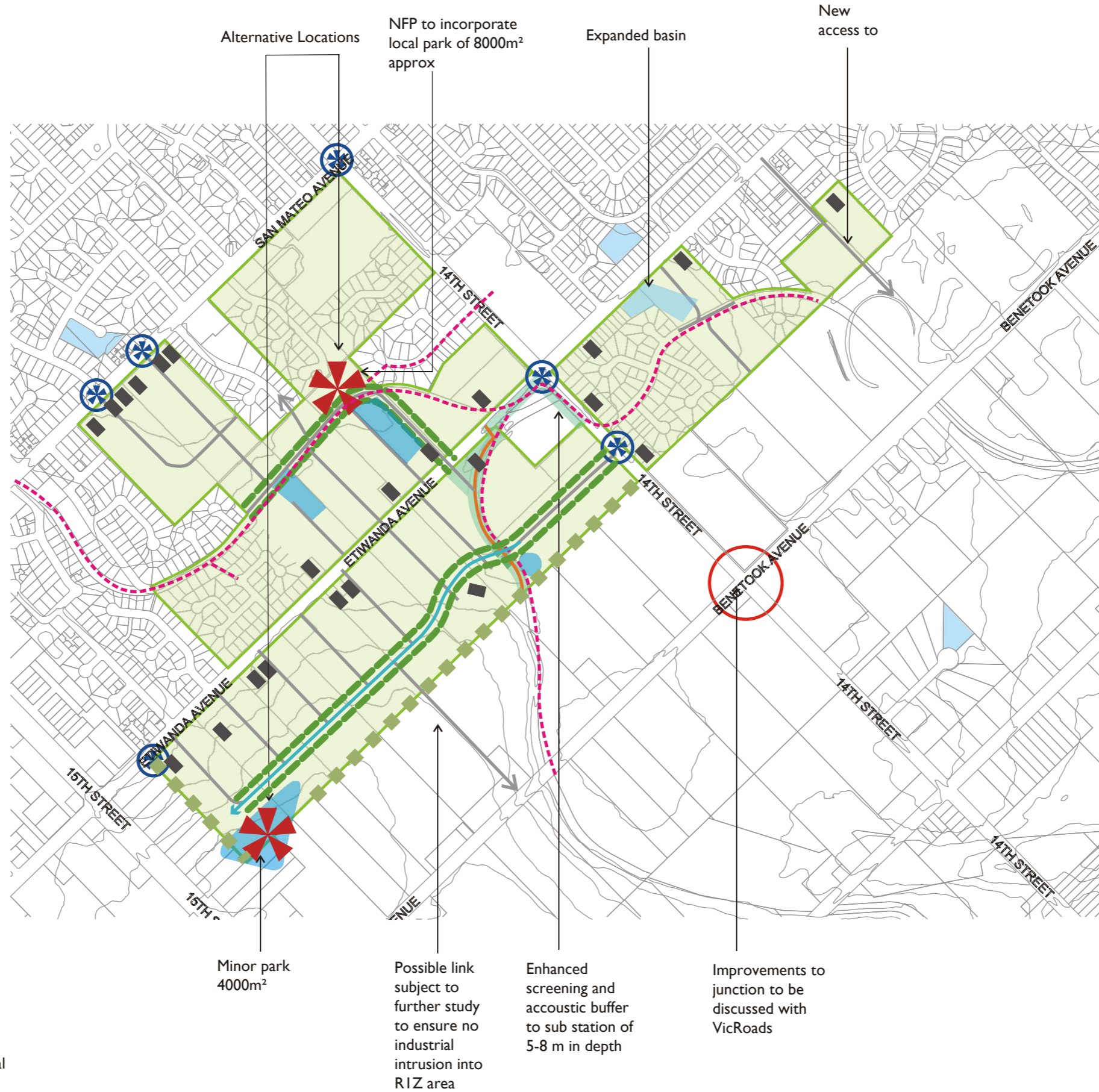


Section 5

Residential Development Plan



-  Key Traffic works
-  Neighbourhood Focal Point
-  Blueway incorporating avenue (30m wide)
-  Avenue/Boulevard (25-30m wide)
-  Standard Road
-  Shareway
-  Gateway Feature
-  Stormwater Management and Public Open Space
-  Site
-  Other Landscape Improvements
-  Pedestrian/Bike paths link
-  Shared buffer between residential and B4 to consist of 2m high wall of significant mass & building setbacks of at least 20m on both sides.  
Note - setback can accommodate outhouses, swimming pools, etc and wall to be constructed as part of the industrial development.



Please note exact alignment of roads may vary in response to detailed site investigations and staging considerations. However variations from this plan will be required to demonstrate how they respond to the issues and principles described in this plans and achieve at least an equivalent standard of design.





Section 6

The Concepts

Indicative Components of the RDP



## High quality streets and open spaces

The streets and open spaces of the study area will need to reconcile a wide range of functional and aesthetic requirements whilst also responding to the issues raised by the community and important stakeholders. The streets and open spaces in this section of the report identify how these changes can be met on the ground. In particular the streets and open spaces are also designed to support “water sensitive urban design”. This allows the drainage infrastructure to be used as an aesthetic asset that naturally supports and irrigates a high standard of landscaping.

This ensures the streets and open spaces are not just good for moving through but also look good as well as well providing attractive safe places to walk, cycle, chat to friends and play. In short they are designed to be places to stay rather than just spaces to pass through.

This section describes some of the key streets and open spaces that will help achieve this goal.

Part 1 describes the common features to all the areas, these are; entrance features that provide an attractive threshold to the neighbourhood and tell people they are entering a residential area and a distinctive neighbourhood. This section also describes the characteristics of stormwater management areas that ensure they are aesthetic assets and are environmentally better than existing drainage basins.

Part 2 describes the streets and open spaces that will be found in this area that will ensure that those streets and spaces are tailored to the needs and values of the community and the circumstances and character of the neighbourhood.

Please note that whilst the sections illustrate how a high standard of design can be achieved here to meet these objectives, it is recognised that the dimensions may be able to be varied where it can be demonstrated that the standard of design can be achieved by an alternate design.

Meeting this standard will require:

- That the long term survival of the landscaping required will not be disadvantaged by an alternate design
- Where adequate room is given to footpaths and cycle paths
- Where the road pavement does not dominate (generally accepted to be around 1/3 of the total width)

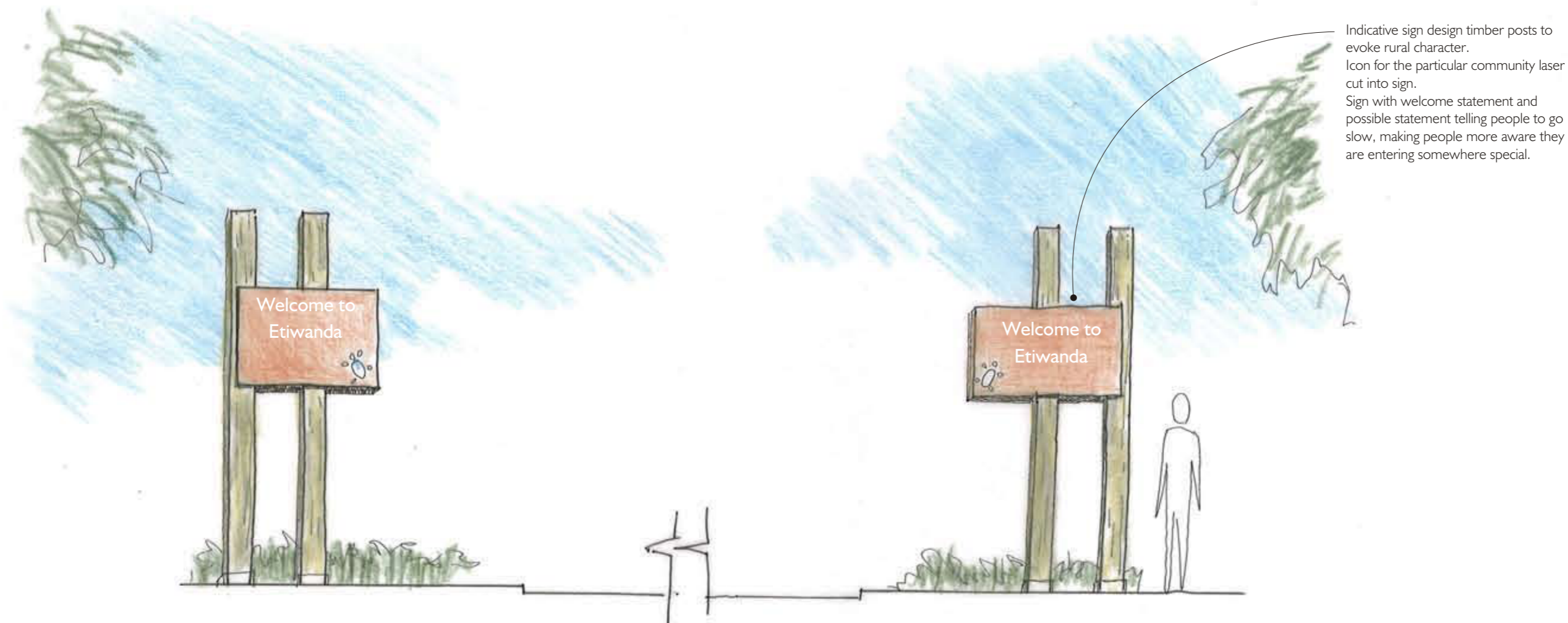
## Part I

### Gateway feature

Gateway features will provide a memorable and distinctive threshold to the area that tells people they are entering somewhere special and tells vehicle drivers they are entering into a residential area. Indicative plan showing one way of providing such a gateway feature

Characteristics:

- 3m high sign which has artistic merit and says something of the area it provides the gateway to
- Feature set in landscaped area to provide attractive composition of structure and landscape - place.





## Part I

### Stormwater management and open space areas

Stormwater management areas will provide an aesthetic and ecologically responsible way of dealing with drainage. This indicative sketch shows one way of providing such a drainage area

Characteristics:

- Shallow banks of varied slope - Curved rather than straight edges where appropriate
- Shallower and larger rather than deep and sm. aller - Planted with trees and indigenous understory planting



## Part I

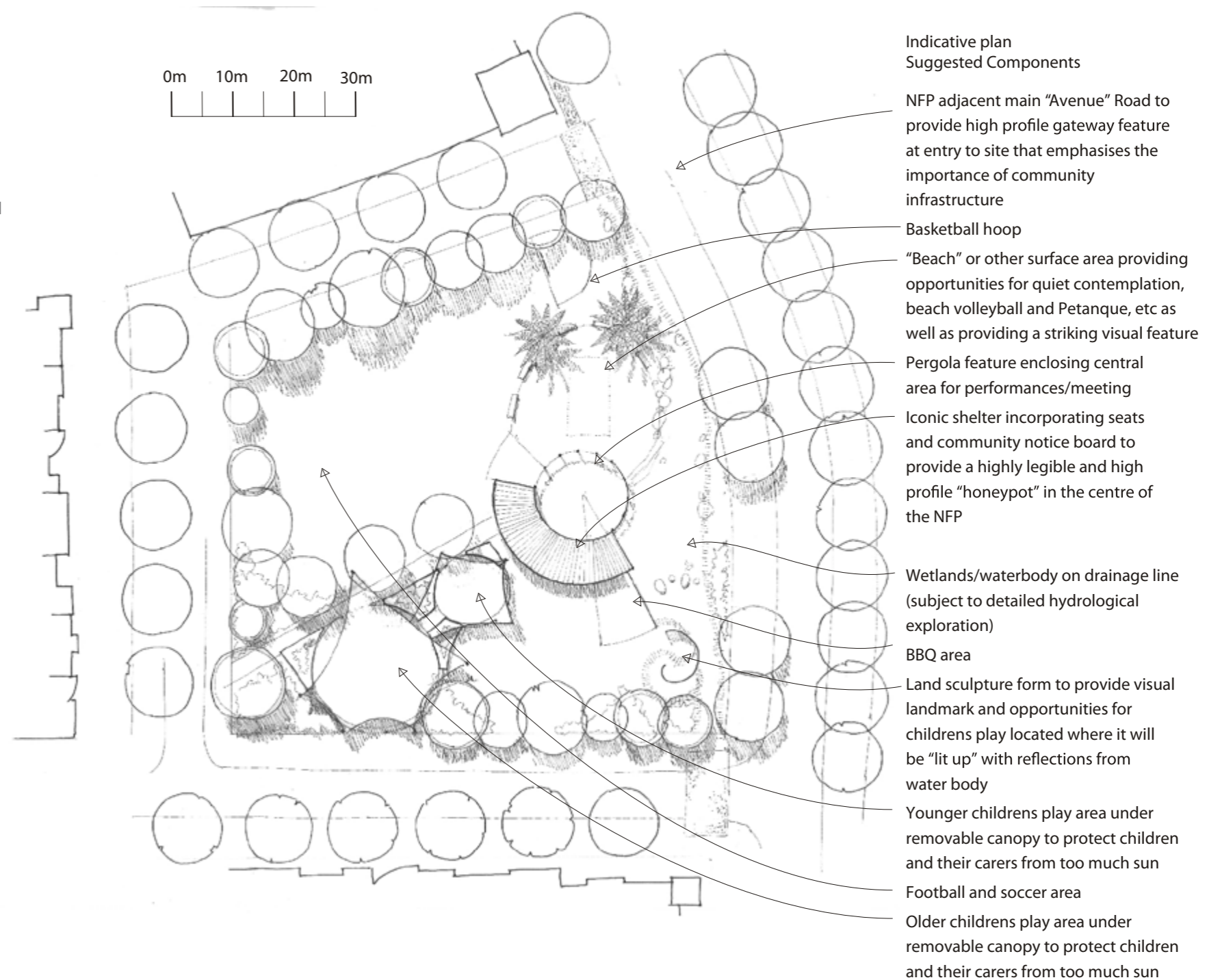
### 'Village Green' - Neighbourhood Focal Point

The NFP provides the social focus for a neighbourhood and supports a wide range of interactions necessary for the suburb to become a community. They integrate high quality open space with appropriate built form to evoke a 'village green' character.

Key features are:

- Offers passive recreational amenity for all cross-sections of the community, old and young; Incorporates social infrastructure may include a barbeque area, play equipment, shelter, seating, tables and a community notice board.
- Houses fronting Village Green are townhouses are built with a zero setback to the side boundary on at least one side and have a small front setback
- Vehicular access of lots fronting village green generally from rear lane to ensure open space is not dominated by garages

Note: Focal Points do not require roads on all four sides, but they do need to ensure houses front towards them on all sides.



## Part I

### Village Green Perspective

Perspective illustrating "village green" character

This illustrates how the village green will be designed to facilitate a wide range of play and social activities and will provide an attractive feature of which the whole community can be proud.

Townhouses could potentially cluster around the village green to provide a sense of the green being an important place and will ensure there will be many eyes on the street to make the village green feel safer. Trees shown at approx 10-15 years maturity. Play areas may also incorporate a removable canopy to provide adequate shade (not shown here).



## Part 2

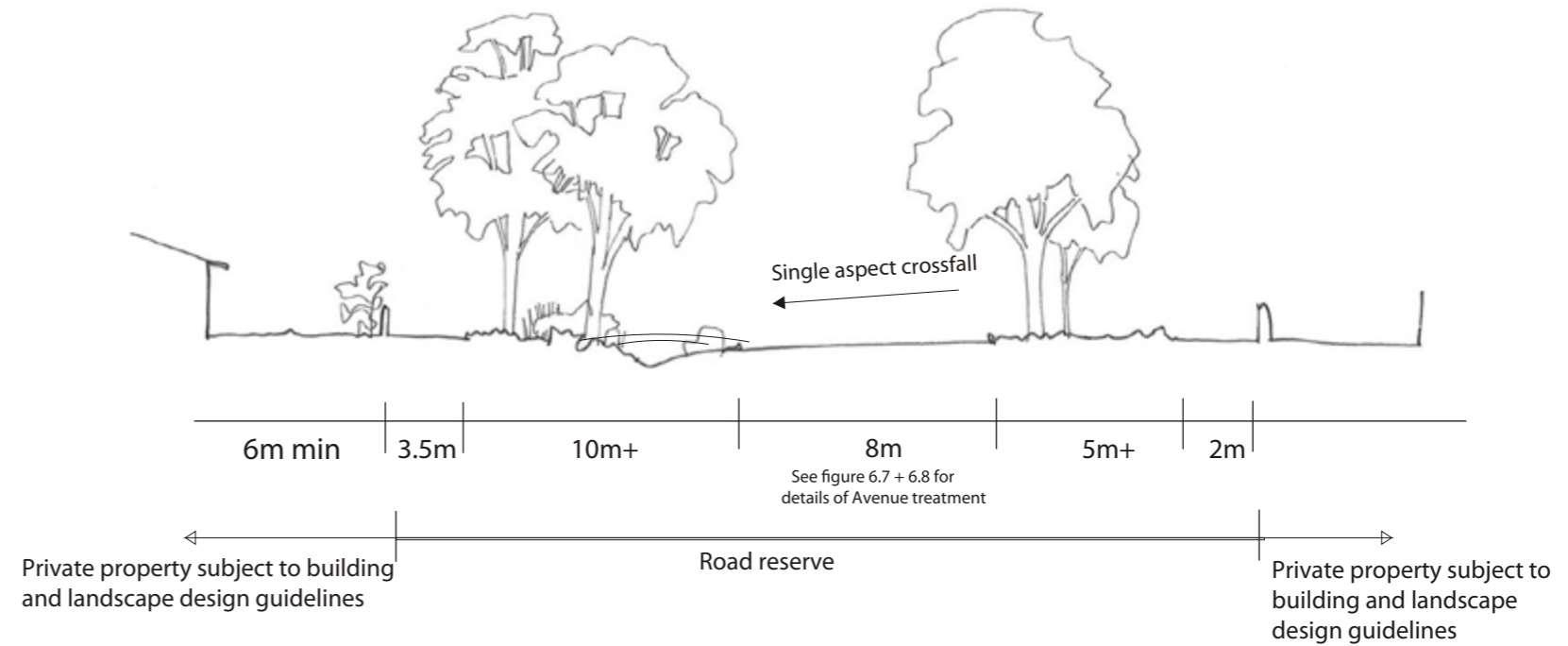
### Blueway

A blueway is a road corridor incorporating a drainage line. The drainage line is designed to be an aesthetic asset that enhances the character of the area and addresses downstream drainage issues in an environmentally sensitive way. It will not look overly engineered.

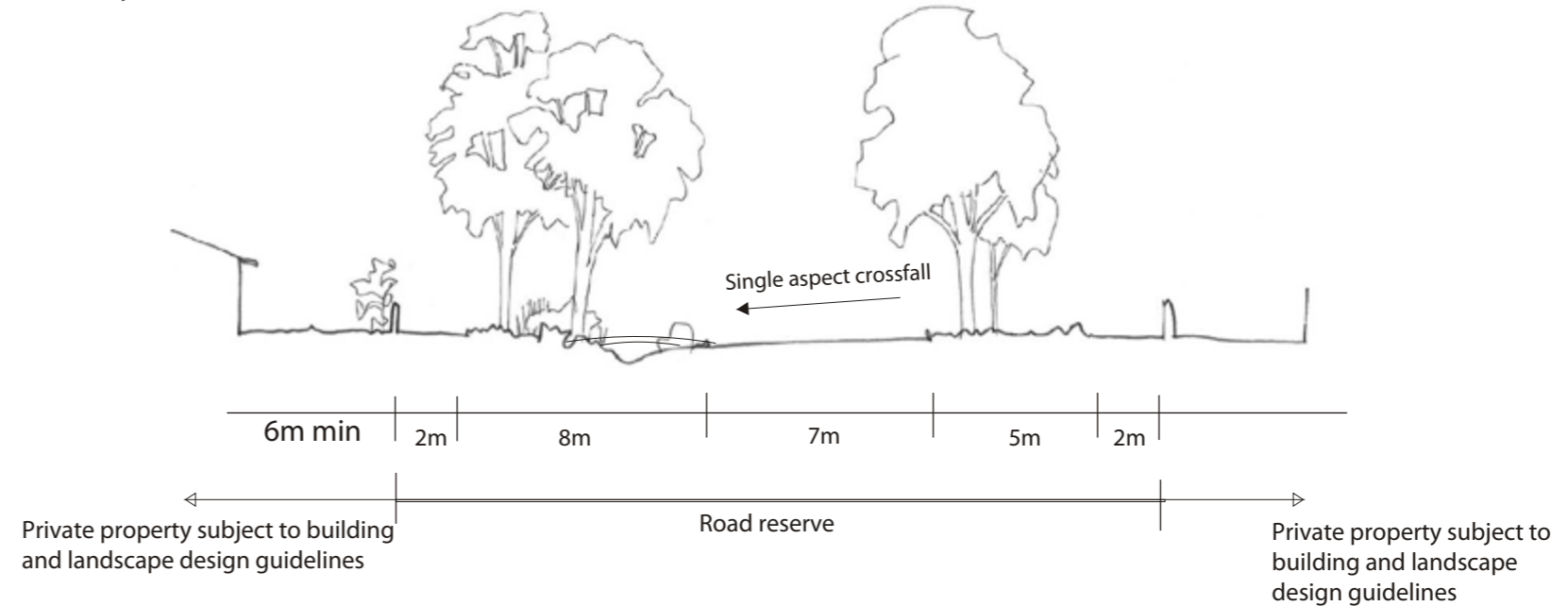
Characteristics:

- 24-28.5m (larger when blueway is accommodated in avenue) ensure the blueway is not dominated by the road. The area dedicated to landscaping will ensure the blueway has an attractive sylvan character.
- The blueway accommodates a naturalistic swale on a drainage line to provide an attractive landscape feature and address downstream drainage issues by facilitating on site detention and infiltration
- Significant landscaping will offer shade and shelter for the adjacent footpath widening to facilitate a safe, comfortable walking route.
- Indigenous planting or in accordance with MRCC Environmental Services Policy
- Engineering components, roads, footpaths and kerbs to satisfaction of MRCC local polices.

### Blueway incorporating Avenue



### Blueway elsewhere



## Part 2

### Blueway Perspective

Perspective illustrating how the blueway will be designed to evoke a dry creek character. This illustrates how the drainage swale may be landscaped to reflect its function as a water channel, even if it only carries water occasionally.

This could provide an important landscape asset for the area that could help establish a strong and attractive character for the area.

The swale also facilitates ecologically responsible drainage which should minimise the need for downstream works, minimise the need for irrigation and increase habitat value on site

The crossings shown are indicative and seek to illustrate how they can reinforce a sense of the blueway being a channel. Other crossings may be appropriate that articulate a crossing.



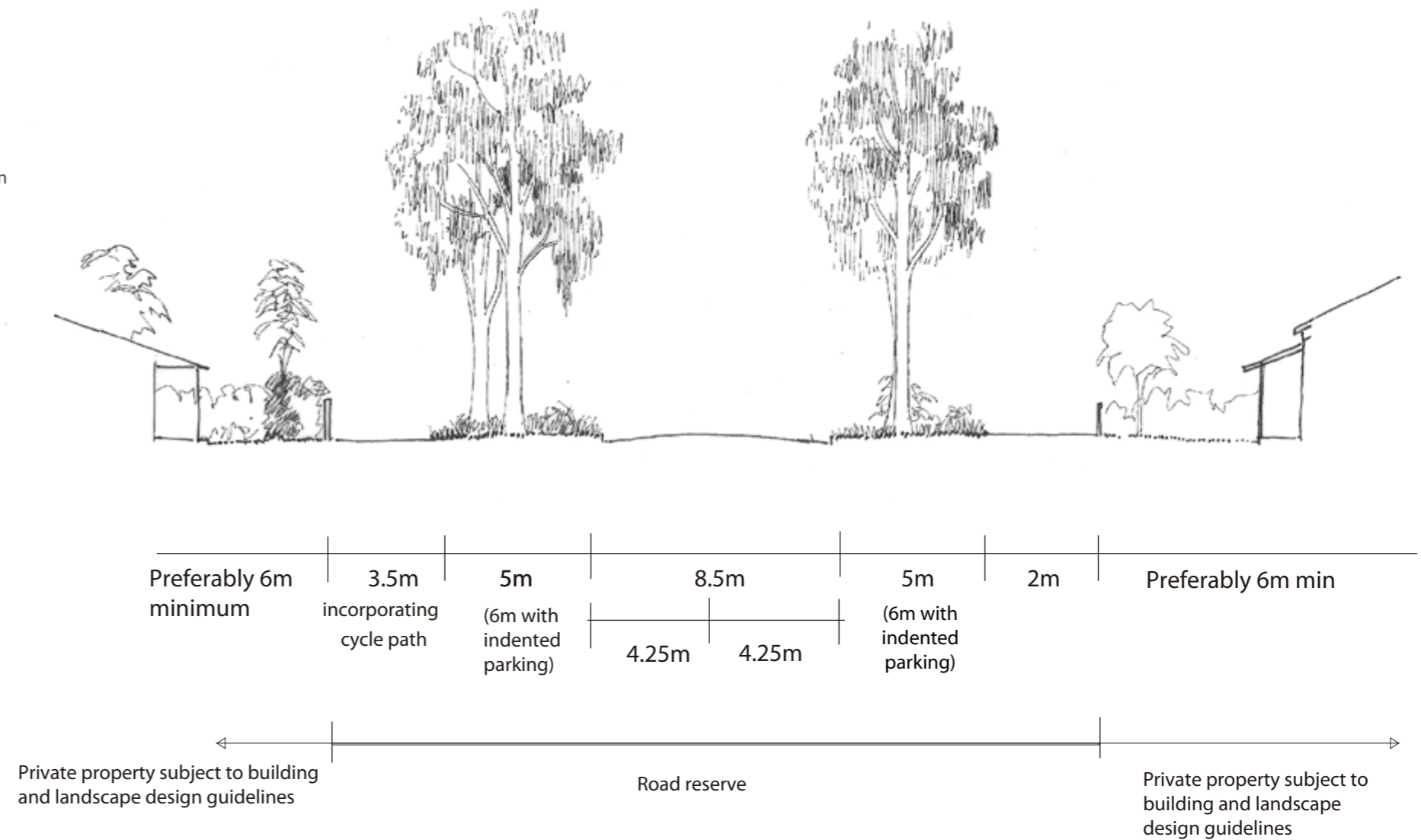
## Part 2

### Avenue/Boulevard

Avenue/Boulevards will provide the main spines through the development area. They will be designed to ensure that the landscape makes a strong contribution to the area's character vegetation.

Characteristics:

- 25-30m wide road reserve (wider with indented parking bays)
- Capable of accommodating a bus route and a cycle path on one side.
- Accommodates 'structural landscaping' in the wider nature strip.
- Capable of accommodating indented parking Indigenous planting or to MRCC specification The significant landscaping will offer shade and shelter for the adjacent footpath to facilitate a safe and comfortable walking route,
- Engineering components, roads, footpaths and kerbs to satisfaction of MRCC local polices.



## Part 2

### Avenue/Boulevard Perspective

This illustrates how the avenue treatment will enhance the areas character and emphasise the importance of the key routes through the development. The significant landscaping will have a better chance to thrive because of the wider nature strips and in doing so ensure the area has a green skyline that is not dominated by a roofscape. This will help develop character of a more urban environment.

The trees are shown at 15 years maturity.

Note: Road reserve shown at 25m as no indented car parking shown.



## Part 2

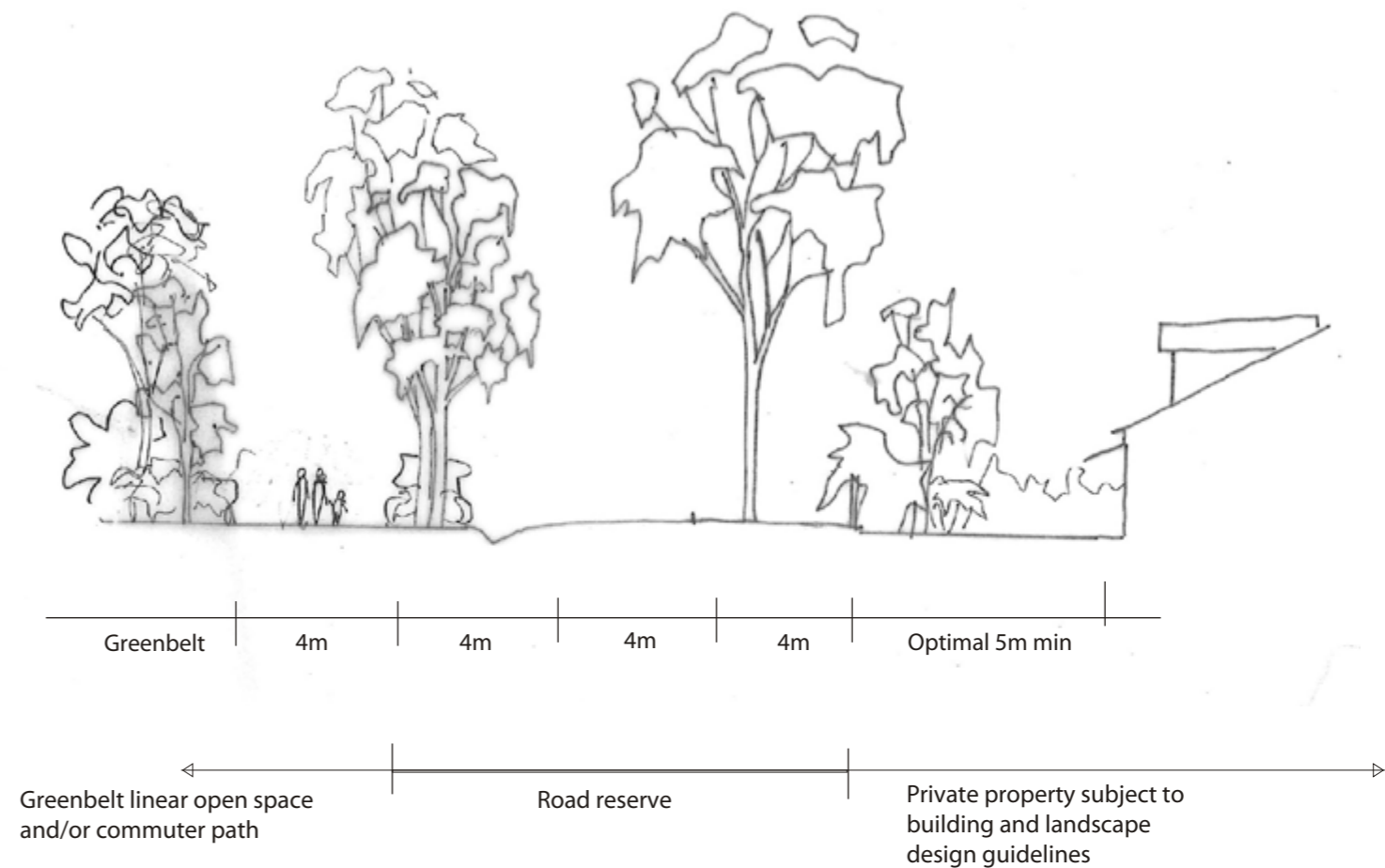
### Shared Pathway/Laneway

Shareways will provide the edges to the green belt. They will ensure the greenbelt is safe and overlooked to ensure they can remain attractive places for walking and cycling.

The shareways can accommodate local vehicular access

Characteristics:

- 12m wide road reserve
- Accommodates walking, cycling and local vehicular access in shareway within landscaped corridor which incorporates retained and proposed trees
- Road course narrows and meanders to ensure slow vehicle speed
- Capable of accommodating indented parking
- Indigenous planting or to MRCC specifications
- Single aspect crossfall, swale and no kerbs to evoke rural lane character .
- Maximum of six houses accessed
- Engineering components, roads, footpaths and kerbs to satisfaction of MRCC local polices.





## Part 2

### Shared Pathway/Laneway Perspective

Perspective illustrating shared pathway/laneway character .

Shared pathways/laneways reconcile limited vehicle access with the imperative to create safe, attractive, overlooked corridors to make walking and cycling relatively more attractive than alternative modes of transport.



## Part 2

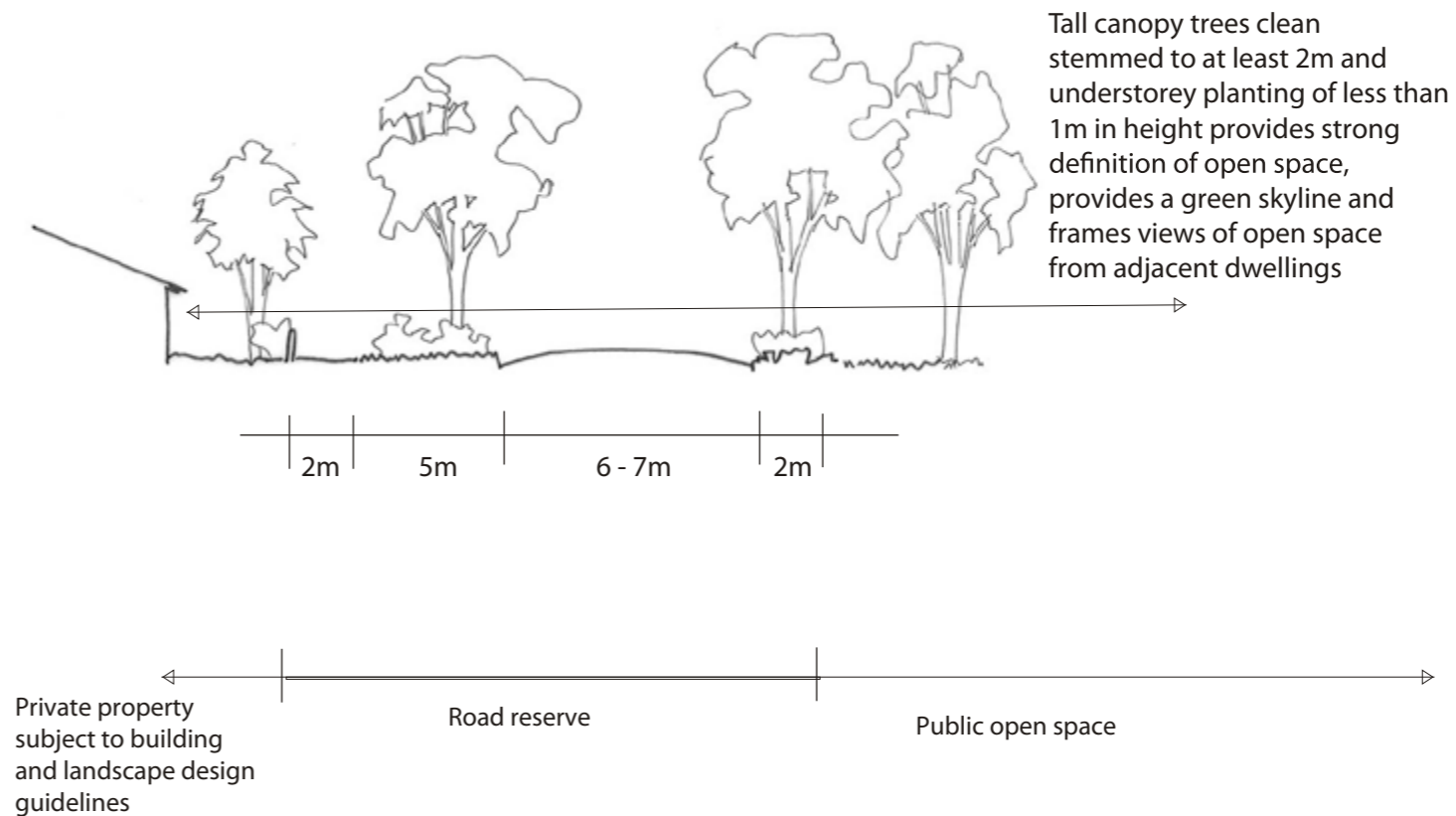
### Edge Road

Edge roads provide the edges to open space. They will ensure the open spaces are safe, overlooked and will facilitate walking, cycling and local vehicular access

Characteristics:

- 15-16m wide road reserve
- Well landscaped edge between open space and residential development.
- Edge roads allow for views from adjacent residences to open space.
- Indigenous planting or to MRCC specifications Engineering components, roads, footpaths and kerbs to satisfaction of MRCC local polices.

Mildura Residential Development Plans 6.II Key Components - edge road  
Nts 030306



## Part 2

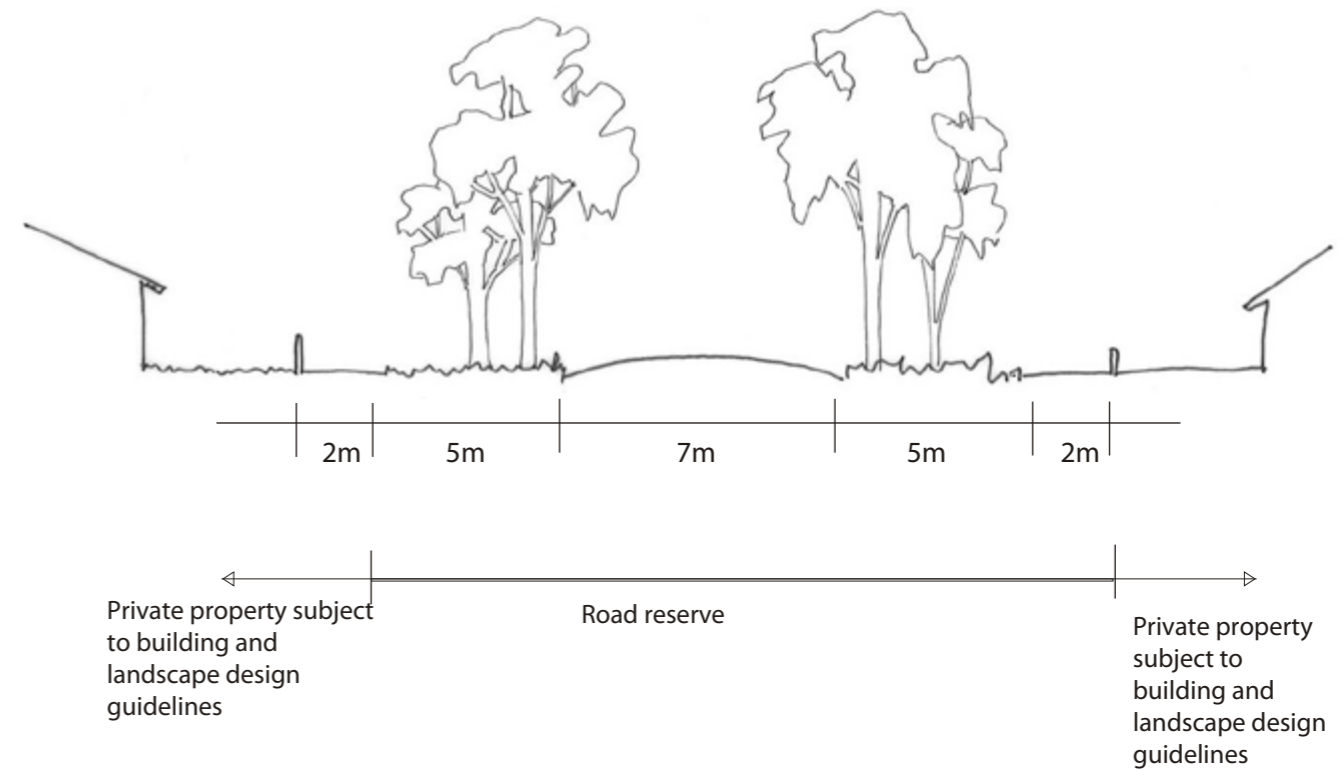
### Standard Road

Most of the roads in the area will be standard roads. The roads will be designed to provide a green skyline and foreground, helping to evoke a sylvan character. This character is further strengthened because the actual road pavement is approximately a third of the road reserve which will ensure the road does not dominate.

Characteristics:

- 21m wide road reserve
- Allows for significant vegetation
- Possible car parking in bays between trees.

Engineering components, roads, footpaths and kerbs to satisfaction of MRCC local policies.





Section 7

The Concepts

Design Guidelines



## Design Guidelines

We envisage that the sensitivity of the environment and the imperative to achieve a high standard of development requires that design guidelines cover the following aspects of development:

Undertaking a site analysis

House type;

- Building setbacks;
- Building height;
- The front of the house;
- Garage and car port design;
- Fences;
- Landscape; and
- Driveways/crossovers.

The guidelines should be promotional in character and seek to explain not just what needs to be achieved but why. The guidelines should include examples of appropriate development and include both requirements and suggestions. The scope of the guidelines has been drawn up to ensure nothing is controlled unless it is essential and everything that is essential is controlled. The guideline requirements express what would be acceptable for that design element but to enable the applicant to make a case for non-conforming proposals as long as they could explain how their solution better met the objective of the guideline.

The suggested content of the guidelines, objectives and key points for all four areas considered in this study are illustrated in Table 1. The table envisages three types of lots that each have guidelines prepared for their particular circumstances. Type A lots are only present to RIZ zoned sites adjacent to a neighbourhood focal point/village green. They have controls to ensure that these houses provide an appropriate edge to these important open spaces. Type B lots are other RIZ lots. Type C lots are the residential lots in the LDRZ area

Table 1 Urban Design Guidelines

<b>Element</b>	<b>Objective</b>	<b>Requirements</b>	<b>Suggestions</b>	<b>Notes</b>
The aspect of design covered by the guidelines	What that aspect should achieve	The specific development requirement that would be deemed to fulfill the objective of the guideline	Other issues that would be beneficial to be considered at the time of design but cannot be required	
Site analysis	Ensure the building design responds to the particular circumstances of the site	Applications to incorporate description of the following aspects of the site; Site orientation Vegetation Drainage lines Views and view-sheds (where appropriate)	Application to incorporate a statement saying how it responds to the aspects listed in requirements.	
House type	Ensure the house type is appropriate for the location	Lots adjacent to neighbourhood focal point to fulfill specific guidelines identified for lot type A.  Lots zoned LDRZ to fulfill specific guidelines identified for lot type C.  Most house lots do not require the controls relevant in these locations and so are subject to a reduced set of specific guidelines identified for lot type B. (see RDP)		
Building Setbacks	Ensure buildings facilitate good surveillance of the public realm and can facilitate significant planting adjacent to the public realm	Type A lots: Front setback 1-3m side 0m  Type B lots: Front setback 5-7m side min 2m All lots outside of the drip line of existing trees identified to be retained  Type C lots: Front setback min 10m side min5m		In case of conflict with above guideline the guideline higher up will take precedence.  Setbacks defined
Building Height	Minimise intrusion on the landscape Minimise intrusion on surrounding properties Ensure equitable view sharing	Type A lots 2 storeys up to a maximum height Type B lots 1-2 storeys up to a maximum height 1-2 storeys up to a maximum height	Encourage useable space within the roof-form.	Maximum height defined to avoid over development or loss of solar access.
The front of the house	Provide adequate passive surveillance of the public realm Facilitate social interaction	Ensure front door faces street Ensure at least one habitable room window faces the street Ensure all house fronts facing north have a verandah at least 1.5m in depth over at least a third of the width of the house-front	Encourage outdoor sitting space in front of lots with other orientations	

<b>Element</b>	<b>Objective</b>	<b>Requirements</b>	<b>Suggestions</b>	<b>Notes</b>
Garage and car port design	Minimise dominance of garages Provide adequate passive surveillance of the public realm	Type A lots Ensure garages are not visible from the primary street frontage Other lots Ensure garages are not forward of the house front.	Incorporate garages into the building form Garage doors no more than 6m wide	
Fences	Contribute to the character of the public realm Minimise spread of wild fire	Ensure front fences are no more than 1.2m high Ensure vegetated fences in front of the dwelling are broken by gaps of at least 5 metres every 20m linear length of frontage		
Landscape	Contribute to the character of the public realm Maximise habitat value Maximise the amenity enjoyed within houses on the land	Type B and C lots Require at least one canopy tree that will grow to a height greater than the house on all lots Ensure all planting is indigenous apart from those immediately to the north of a dwelling which should be deciduous to facilitate good solar access in winter	Council to provide list of preferred plants	
Driveways/ crossovers	Minimise intrusion into the landscape Minimise impact of development on the areas hydrology	Type B and C lots Driveways no more than 3m wide at entry to block Crossovers radii to accommodate vehicles with trailers	Minimise area of impermeable surfaces Construct driveway from permeable material such as granitic sand or Lilydale toppings where possible.	



Appendix I

Summary of Consultation Feedback



Area/observation	Source	Agree?	Disagree?	Comments
<b>Etiwanda</b>				
Current overlays/zones AEO2,DCPO,DPOI, SMO,RIZ				Confirm is revision required – Assets department
Site crossed by former pipeline	rc	Yes		Check easements as some have changed DG>Existing FMIT irrigation pipe in approximate channel reserve area.
Influenced by substation and FMIT infrastructure	c	Yes		Confirm status of FMIT depot and future plan
Desire line to TAFE across private property	c	Yes		
Truck depot across Benetook Avenue will cause interface issues	c	Yes		Confirm with VICROADS main traffic route for trucks – understand its Benetook and not 14th
Prominent substation partially screened	c	Yes		More screening req?
Poor connectivity between existing subdivisions and area yet to be subdivided	c	Yes		Perhaps approach developer to get access way before land is sold? Perhaps approach developer to get access way before land is sold? DG> There is some access to Matthew Flinders Drive from Ex. Dev (James Harwood)
Drainage area cross several boundaries	c	Yes		Staging from “bottom” to “top of catchment?”
Residents keen to get started on the process	r	Yes		
Existing Market garden – owners suggested they will not develop to housing in short term				
FMIT depot “here to stay”	c			Confirm for how long Do we need to know DG>FMIT depot to remain. FMIT may subdivide Ex. House. They have no problems allowing the ped. Link to go through depot to allow ped access to St Pauls school.
Benetook Avenue/14th st “worst junction in Mildura”	r/c/sk	Yes		Can it be improved DG>MRCC to carry out detailed analysis / options.
V deep and engineered detn basin adj Etiwanda Ave.	c	Yes		Can it be improved DG> Vinian subdiv. Basin to be expanded to allow vacant areas to be developed. MRCC infrastructure very very limited.
Fragmented area	c	Yes		
Drainage issues	r/c	Yes		DG> MRCC has concepts estimated. MRCC to define the specific areas for wetlands.
Traffic management onto etiwanda/san mateo	sk	Yes		
Future needs for family and children services in yet to be developed areas future needs difficult to estimate	co			Relationship to SUZ in Interface study survey form response
Staging – ability to cut of house and land -	sk			DPO schedule & req for planning scheme amendment?
Existing outline DP's – what is status	sk			Adopted with recommendations to consider points raised in exhibition process prior to finalising – that is part of this process
Should there be links between industrial/B4/residential	sk			
Buffer with proposed B4 (C29)	R/sk			is it on B4 land is it shared – what other functions could it perform
Land on cnr Benetook /14th part of C29				Proposed to be rezoned to B4 application in for truck facility – strong opposition from local landowner

Area/observation	Source	Agree?	Disagree?	Comments
Constraints of approved subdivisions	c			
Consideration of interface study	co			
Unit development exempt from notice if approved development plan	co/r			Can the RDP have guidelines as impact on adjoining landowners can be negative
Consideration of grid layout of streets supported	co			
Bike path route	co			How does it connect to o/a mrcc bicycle plan
Salinity issues – land covered by SMO & in schedule to DPO	Sk			Council have resolved a publicity campaign is preferable in lieu of s173 on water wise issues
Portion of a property land-locked on etiwanda	sk			Land extends towards Benetook but no access from benetook
Consideration of sustainable housing				
Land extends towards Benetook but no access from benetook				<p>DG&gt; Most existing houses to remain and be subdivided by owners prior to development. Permit conditions to reflect lots to be fully serviced by developers.</p> <p>Specific permit conditions?</p> <p>DG&gt; One allotment spans over both RIZ and B4 land. Planning permit to allow subdivision. Specific permit conditions?</p> <p>Sewerage system – generally towards back of Bunnings.</p> <p>Buffer on B4 land or RIZ?</p> <p>(Likely to be on B4)</p> <p>Bicycle / Pedestrian links – along old channel res (approx)</p> <p>Community Focal points – like some new areas in Melbourne.</p> <p>(would not have to be shop, just community area)</p> <p>Public transport and traffic issues. Limit access to collector roads and allow distrib. Of traffic to all sides?</p> <p>Grid system to roads.</p> <p>Body Corporate opportunities for some constrained / mid sized lots.</p> <p>Green belt – unique concept to be maintained.</p>

R = Resident

C = Consultant

SK – Stakeholder

CO – Council officer

DG=Danny Grazan (GHD)

Appendix 2

Summary of Consultation Amendments



## Appendix 2:

### Summary of Consultation Amendments

This appendix outlines the feedback received as a result of the workshops held on the 31st March and the 1st April. After these workshop sessions a final draft concept was prepared with the appropriate ideas amended. The following documents the community, stakeholder and council feedback and explains why a change was or was not appropriate to the concept.

#### Etiwanda:

<b>Actions and Agreements</b>	<b>Consultant Response</b>
Recreational facilities identified in the short term that would service the new residential growth in Etiwanda include the following works at No 1 Oval: <ul style="list-style-type: none"><li>• Playground facilities (major, includes toilets and car parking – (Assets to confirm)</li><li>• 2 x netball courts</li><li>• Refurbishment of the ovals (Assets to confirm if this can be included)</li></ul>	Noted
Links to Irymple greenbelt should be incorporated in line with the Mildura Bicycle Strategy.	Agreed and amendment made
Focal Point at FMIT Depot to be removed. Focal Point behind Plane Tree Drive to be a major park, Focal Point to south should be a minor park.	Agreed and amendment made





Appendix 3

Summary of Amendments in Response to Submissions made at Exhibition



Note:

These submissions cover all four areas of the RDP. This is because some of the submission refer to more than one area.

## RESIDENTIAL DEVELOPMENT PLAN

List of Submissions received at closing Monday 27 November 2006:

S/No	SUBMITTOR	DATE RECEIVED
17	SD & E Pica	8 November 2006
10	T & S Violi	13 November 2006
18	K. Steinle on behalf of Horsfall family.	23 November 2006
1,5,6,7,16	Lower Murray Water	23 November 2006
14	Freeman & Freeman – Kedmenec Property	23 November 2006
15	Peter & Wally Kedmenec	27 November 2006
13	Roy Costa on behalf of I & M Dimasi	23 November 2006
12	Roy Costa on behalf of G. Capogreco & M T Nesci	23 November 2006
4	Freeman & Freeman on behalf of Mr Leng	27 November 2006
11	Thomson & Singelton on behalf of Messrs. Ghidinelli & Dimasi	27 November 2006
9	M. de Maria	27 November 2006
3	B. Scott	27 November 2006
8	Powercor	27 November 2006
2	Freeman & Freeman – landowners 15th Street b/n Riverside & Ontario Avenue	27 November 2006

SI	RESIDENTIAL DEVELOPMENT PLAN – 15TH ST BETWEEN ONTARIO AND RIVERSIDE AVE	
Submitter	Lower Murray Water	
Issues	NA	
<b>Submission summary</b>		<b>Response and Recommendation</b>
This report appears to accurately reflect the comments conveyed to Freeman & Freeman at the consultation stage and we have no further comments to make at this point in time.		N/A

S 2	RESIDENTIAL DEVELOPMENT PLAN – 15TH ST BETWEEN ONTARIO AND RIVERSIDE AVE	
Submitter	Freeman and Freeman	
Issues	<i>Greenway design and location</i>	
<b>Submission summary</b>		<b>Response and Recommendation</b>
The proposed road reserve widths of 25-30 metres for the boulevard/avenue and 21 metres for the standard road are excessive, too far removed from ResCode standards and out of character with the already surrounding area.		<p>The dimensions suggested are required to achieve amenity standards required by the new section 56 of the VPP and in particular ensure the road pavement does not dominate the road corridor. The sections have been designed to allow room for landscaping to provide shade, create an attractive, walkable environment and enhance its habitat value.</p> <p>However it is accepted that if the road pavement can be reduced then so can overall corridor as long as it maintains the overall proportions described in the RDP.</p> <p>The vision plan should be amended to make this explicit.</p>
Development plan to finish at page 18 or permitted to amend the plan so that it is evident that pages 19-31 are indicative only and that narrow roads will be permitted, providing what is proposed at the time of subdivision can be justified as appropriate.		Road design is important in achieving standards required by section 56 of the VPP. Therefore they should be more than just indicative. However the RDP should recognise some variation would be acceptable as long as it can be demonstrated that design standards can be met with a narrower corridor.
Creation of extra wide nature strips and plantation areas are not appropriate due to water issues.		It is agreed that this is an important issue. However it is primarily a function of plant selection and the land would have maintenance demands whether it would be in private land or road corridors. The landscape proposed in the RDP is intended to help mitigate environmental conditions at street level and so help reduce evaporation
Concern with Etiwanda Avenue boulevard is long and straight and makes no effort to achieve any reasonable solar orientation and the general move is away from court bowls.		<p>This layout was designed to achieve an efficient lot layout, facilitate a straight road to village green which is the shortest and most walkable route and to tie all the areas tighter given shape of site.</p> <p>The concept layout does not suggest court bowls</p>

<b>S3</b>	<b>RESIDENTIAL DEVELOPMENT PLAN – NICHOLS POINT</b>	
<b>Submitter</b>	<b>Barry Scott</b>	
<b>Issues</b>	<b><i>Supports overall concept generally, concerns regarding existing amenity and safety issues of circuit</i></b>	
<b><i>Submission summary</i></b>	<b><i>Response and Recommendation</i></b>	
Comments provided during the consultation process do not seem to have been taken into account.	Were considered but may not have been applicable to incorporate.	
Values of tranquillity and quietness that existing residencies with long setbacks enjoy will be compromised by the introduction of the continuous road (standard or blueway) adjacent to properties	Roads are designed to be slow speed and be attractive. The circular route around the township is not intended to be continuous road which will minimise intrusion.	
The values that existing owners enjoy and the cost at which those values come should be considered when future planning is undertaken.	Agreed, that is important and residents will have the right to make submissions at the Development Application stage	
The need to make any new roads continuous is not demonstrated in the plan. The roads north and south of the site plan are designated as not being continuous but are to have a greenway and continuous access for pedestrian and cycle traffic. This principle should be applied in a consistent manner regardless of the presence of restrictive infrastructure.	No need for continuous road (we don't want people to drive all the way around the circular route) but people should be able to walk or cycle around.	
The net result of having discontinuous roads will be prevented thus making the roads safer and possibly reducing traffic volumes in some areas while still providing the concept of a continuous greenway with pedestrian and cycle routes.	Noted.	
The plan needs to take into account that existing houses may be aligned to existing roads and therefore they will not be aligned to new roads and may not comply with the designated 10m setback.	Agreed, neighbours will be consulted at the Development Application stage. The RDP recognises that slight variations may occur at the Development Application stage to accommodate site specific issues such as this. This should be made explicit in the plan.	
Existing residences may have a road reserve 5m from the residence and side fences will adjoin the reserve thus affecting some of the key design concepts of the roads.	Noted requires further investigation at the Development Application stage	
Where existing properties exist, any adjoining non-continuous road should be sited such that the property offsets are taken into account and that a single pathway should be located on the other side of the road as demonstrated in the 'shared pathway/laneway' concept.	Footpath will need to be on consistent side for optimum utility. The side will be chosen for overall amenity.	
The area between the property boundary and road/and or swale would be vegetated with indigenous planting with plant selection being determined by the location of the house into consideration.	Noted	
I would like to take the opportunity to discuss this further and fully explain my concerns.	Will get opportunity to comment at development application stage.	

<b>S4</b>	<b>RESIDENTIAL DEVELOPMENT PLAN – NICHOLS POINT</b>	
<b>Submitter</b>	Freeman & Freeman on behalf of Mr Leng	
<b>Issues</b>	<i>Plan appears to have ignored his concerns previously raised during consultation period.</i>	
<b>Submission summary</b>	<b>Response and Recommendation</b>	
<p>Objections to</p> <ul style="list-style-type: none"> <li>the very wide “ through” road greenway/blueway within his property, due to effect on reducing yield from 9 to 8 house lots. A maximum road width of 16m is required to achieve the yield.</li> <li>The bend in the road within his property. Belives the road should be straight and be staggered from the proposed road on the other side of Koorlong Ave.</li> </ul>	<ul style="list-style-type: none"> <li>The co-ordinated masterplan approach for the whole area embodied within this study gives certain routes strategic importance which is reflected in the detailed design. The greenway/blueway has strategic importance and therefore needs a design emphasis which requires the wider corridor</li> <li>However it is accepted that it can be narrowed where design objectives can be demonstrated to be met. In particular if road pavement can be narrowed then overall width may be reduced accordingly. There is less scope to reduce the nature strip given its role to accommodate significant tree planting, underground services and in some instances drainage</li> <li>Footpaths of the width suggested are required to facilitate and encourage walkability</li> </ul>	
General concern about all of the cross roads indicated on the Vision plan.	Needed to achieve design standards and ensure best chance for long term survival of street landscaping.	
Gateway Features – are they required as new signs have only recently been erected?	Investigate location and quality of these new signs.  Refer DCP#2 for project details.	
Would like to see that the road reserve widths are indicative only and open to negotiation with Council at the time of planning a subdivision.	Street character is important to achieve section 56 objectives and other planning and design characteristics. This means creating streets that are not dominated by roads and cars and are attractive, pleasant, safe places to walk. This requires room for landscaping and footpaths and road pavement to generally occupy no more than around a third of the road corridor. However it is recognised that these dimensions many change on a site to site basis where appropriate. RDP should be amended to explicitly recognise this point.	

<b>S5</b>	<b>RESIDENTIAL DEVELOPMENT PLAN – NICHOLS POINT</b>	
<b>Submitter</b>	<b>Lower Murray Water</b>	
<b>Issues</b>	<b>General issues on technical aspects of RDP</b>	
<b>Submission summary</b>		<b>Response and Recommendation</b>
Issues Plan – a pump station has been indicated at the southern extremity of the site but ownership or purpose has not been identified.		Site wastewater treatment system to be resolved by LMW / MRCC – see next point
Stormwater & Wastewater Management Plan – the matter of wastewater management has not really been addressed, but this is understandably given that Nichols Point has been included in the Country Towns Water Supply and Sewerage Programme and the concept for sewerage is yet to be approved by DSE. If a pressure sewerage system is adopted each lot is likely to have it's own pump station, therefore the low pressure pump station for blackwater shown at the southern extremity should be deleted from the plan, as indicating it would be misleading.		Delete pump as the provision of the low pressure system will mean each lot requires an individual pump.

<b>S6</b>	<b>RESIDENTIAL DEVELOPMENT PLAN – RIVERSIDE AREA</b>	
<b>Submitter</b>	<b>Lower Murray Water</b>	
<b>Issues</b>	<b>General issues on technical aspects of RDP.</b>	
<b>Submission summary</b>		<b>Response and Recommendation</b>
Stormwater & Wastewater Management Plan – the alternative to a central sewerage pump station on either side of the ridgeline bisecting the site, is a pressure sewerage system with a grinder pump locate don each lot created, at the building stage on the lot. A S/173 Agreement on title would be a planning permit condition to support such a system.		NOTED - Plan doesn't seem to indicate a central pump station?
4.0 Low Density Residential Zone – the discussion under this main heading and under the sub-heading 'Potable Water and Wastewater' has erroneously included a fourth point specific to Nichols Point and should be removed.		Noted will amend report
Vision Plans – All vision plans appear basic and do little to assist in the forward planning of services due to their lack of detail.		The purpose of the Vision plan is to establish a framework and not finalise all details.



<b>S7</b>	<b>RESIDENTIAL DEVELOPMENT PLAN – ETIWANDA AVENUE</b>	
<b>Submitter</b>	<b>Lower Murray Water</b>	
<b>Issues</b>	<b>General issues on technical aspects of RDP.</b>	
<b>Submission summary</b>		<b>Response and Recommendation</b>
Context Analysis Plan – the identification of the LMW land at the corner of 14th St and San Mateo Ave as a transformer station would appear in appropriate. Better identified as an Office & Depot.		Amend page.
Issues Plan – the area comprising the LMW office and Depot is zoned PUZI and is not land that can be residentially developed as the plan seems to indicate.		Plan shows no zoning inside study area on this plan Related to the Mildura – Irymple Interface Study.
Land identified as future B4Z along Benetook now being considered to be rezoned?		
Stormwater and Wastewater Management Plan – this plan should indicate the land south of Etiwanda Avenue will be seweraged to the pump station at the rear of “Tradelink” therefore a sewerage corridor as well as drainage will be required along the “15 chain” boundary.		Plan seems to indicate this
The sewer pump station shown closest to Matthew Flinders Drive does not exist and nor is there any intention to locate one in the vicinity.		Noted - Modify plan to suit
The sewer pump station may possibly be located somewhere toward the FMIT Depot as indicated, unless a sewer can be brought through to Etiwanda Ave from the proposed sewer along the “15 chain” boundary of the properties on the south side of Etiwanda Ave.		Noted – subject to detailed design investigation
Vision Plan – Very basic and would be more helpful if greater detail of indicative road hierarchy was shown with nominated drainage corridors etc.		Will amend to show drainage line.  Would not be appropriate to have too much detail on framework plan. Intention of the RDP is to establish structure and key character.

<b>S8</b>	<b>RESIDENTIAL DEVELOPMENT PLAN – ETIWANDA AVENUE</b>	
<b>Submitter</b>	Powercor	
<b>Issues</b>	<i>Buffers to zone substation.</i>	
<b>Submission summary</b>		<b>Response and Recommendation</b>
Powercor's zone substation is located on the corner of Etwanda Avenue and I4th Street.		Is shown on Vision plan
Due to the substation requiring additional transformers in the future to meet demand higher noise emissions will develop.		Screening of substation can include acoustic buffer treatment.
Powercor recommends that a 5-8m greenbelt buffer zone be established along the substation boundary where it adjoins the residential land use.		Agreed, the plan will explicitly require a 5-8m buffer zone.
The buffer may be in the form of shared pathway or laneway as shown in section 6 of 'The Concepts' to provide visual amenity and a noise reduction to the benefit of property owners in close proximity.		Agreed
When a proposed residential design plan is available for comment we will be in a position to review noise level emissions and a reduction in the greenbelt buffer zone may be possible.		Noted
A developer proposal for a 2m high fence may not be appropriate due to the possible two storey townhouse type development adjacent to the substation,		To be resolved at development application stage.
The visual amenity works by Powercor on I4th Street and Etiwanda Avenue have been correctly identified.		Noted.

<b>S9</b>	<b>RESIDENTIAL DEVELOPMENT PLAN – ETIWANDA AVENUE</b>	
<b>Submitter</b>	M . de Maria	
<b>Issues</b>	<i>Shared buffer zone</i>	
<b>Submission summary</b>		<b>Response and Recommendation</b>
Disagree with the proposal to have a shared buffer zone consisting of a 2m high wall and building setback of 20m on both sides of fences  In preference may I recommend a similar buffer as that of Johnson trucks (B4) AND Brian Reed (R1).		An adequate buffer is required for both adjoining uses. In our opinion a greater buffer than that described in the submission is required to protect residences from noise intrusion and minimise complaints about business uses.

<b>S10</b>	<b>RESIDENTIAL DEVELOPMENT PLAN – ETIWANDA AVENUE</b>	
<b>Submitter</b>	<b>T &amp; S Violi</b>	
<b>Issues</b>	<b>Shared buffer zone</b>	
<b>Submission summary</b>	<b>Response and Recommendation</b>	
Concerns raised at the shared buffer zone located between the Etiwanda Avenue residential land and the B4Z & proposed IN3Z in Benetook Ave.	A buffer is required to minimise conflicts between the two uses. An example of such a conflict is the noise of the business and industrial uses compromising the amenity of the residential land.	
Buffer is indicated as 20m set back on both sides and a 2.0m high wall of 'significant mass' seems excessive.	Buffer as described will protect both uses and avoid future conflicts. Final detail of wall to be discussed at development application stage.	
Johnson trucks on the corner of 14th and Benetook have <ul style="list-style-type: none"> <li>• No wall between the two land zones</li> <li>• No buffer on the RIZ land,</li> <li>• Approx 10m landscaped buffer on the Johnson trucks land.</li> </ul>	Buffer as described will protect both uses and avoid future conflicts	
Our recommendation would be: <ol style="list-style-type: none"> <li>1. That the 2m high wall be deleted from the proposal, and that a landscaped buffer zone to a maximum of 15m either side of the different land uses.</li> <li>2. Retain the proposal for the 2m high wall but reduce the area of landscaped buffer zone to a maximum of 5-10m either side of the different land uses.</li> </ol>	Either option would certainly diminish conflict, but does not address all potential problems.	

<b>SII</b>	<b>RESIDENTIAL DEVELOPMENT PLAN – ETIWANDA AVENUE</b>	
<b>Submitter</b>	Thomson & Singelton on behalf of Messrs Ghidinelli & Dimasi	
<b>Issues</b>	<i>Boulevard being straight, Buffer issues, Drainage issues,</i>	
<b>Submission summary</b>	<b>Response and Recommendation</b>	
Problems are for seen in the following areas:	The reasons why straight roads are typically used are:	
<ul style="list-style-type: none"> <li>Boulevard being straight</li> </ul>	<ul style="list-style-type: none"> <li>Mildura is on a grid</li> </ul> <p>Allows views to be framed</p> <p>Straight roads minimise distances to the village green</p> <ul style="list-style-type: none"> <li>Lot sizes are regular/efficient layout</li> <li>Protect amenity</li> <li>Investigate</li> <li>Road alignment makes for efficient lot layout. Site would not be developed without owners selling/redeveloping.</li> </ul>	
<ul style="list-style-type: none"> <li>20m building setbacks</li> </ul>	<ul style="list-style-type: none"> <li>20m is required to provide at least a degree of buffering between uses. It is intended that this zone can be used for landscaping and outbuildings to further screen the B4Z.</li> <li>This should be made explicit within the RDP</li> </ul>	
<ul style="list-style-type: none"> <li>House on Lot 4 in line with boulevarde</li> </ul>	<ul style="list-style-type: none"> <li>Development will not be forced on any landowner and landowners will have the right to comment on neighbours proposals at Development Application stage. Also note that the RDP concept is expected to vary slightly in detailed design where these issues can be considered.</li> </ul>	
<ul style="list-style-type: none"> <li>Drainage does not appear to follow contours</li> </ul>	<ul style="list-style-type: none"> <li>Our advice is this layout addressed drainage issues</li> </ul>	
<ul style="list-style-type: none"> <li>Problems with shareway along curved boundaries</li> </ul>	Detailed plan subject to variations in detailed design	
<ul style="list-style-type: none"> <li>Not managing a good lot ratio with wide roads</li> </ul>	The road widths suggested are necessary to meet current design standards and will achieve better quality subdivision	
<ul style="list-style-type: none"> <li>Every driveway through a swale drain will need a culvert to allow water to drain to the basin</li> </ul>	Shared driveways will help minimise crossovers, this works well elsewhere.	
<ul style="list-style-type: none"> <li>There may be problems with direct access from RIZ to the proposed B4Z/industrial land abutting on the south east.</li> </ul>	Agreed, the final form of this link will be subject to further studies to ensure the amenity of the residential area is not compromised.	
<ul style="list-style-type: none"> <li>As the old FMIT channel is no longer in existence it may be preferable to realign internal road</li> </ul>	Detailed plan subject to variations in detailed design to consider circumstances that may change between now and the implementation of the plan	
<ul style="list-style-type: none"> <li>The requirement for solar orientation will be a problem with the boulevard – Etiwanda Ave being on 45 degrees</li> </ul>	Agreed that this is an issue but feel need for efficient lot layout more important given the shape of the site and competing objectives of creating accessible community infrastructure for the whole development area.	

<b>SI2</b>	<b>RESIDENTIAL DEVELOPMENT PLAN – ETIWANDA AVENUE</b>	
<b>Submitter</b>	Roy Costa on behalf of G.Capogreco & M. Nesci	
<b>Issues</b>	<i>Detailed design issues, buffer issues</i>	
<b>Submission summary</b>	<b>Response and Recommendation</b>	
Stormwater basin comments as follows:		
<ul style="list-style-type: none"> <li>Will the owners be compensated by Council at a rate for the value of the land as a RIZ is the basin as indicated is established.</li> <li>Will Council construct the proposed basin and associated works with the monies to be contributed by the Development Contribution Plan over the land and no additional contributions will be required.</li> <li>Can you confirm that the owners of the land are able to develop once the development plan is approved, without the requirement of the construction of the basin.</li> <li>Objection to the sewerage pump as indicated on the plan being relocated onto the owners property due to the affect on the agricultural holding.</li> <li>If the basin is required prior to the subject land being developed will Council compensate for the operation impact on the property</li> <li>Will the owners be able to continue operation of the vineyard should this occur.</li> </ul>	To be discussed and finalised as part of the development application process.	
Buffer Area comments as follows:		
<ul style="list-style-type: none"> <li>Confirmation that residential allotments can be created between the boulevard and rear boundary.</li> <li>Confirm if outbuildings can be located within the buffer – 20m is to the rear of the dwelling only?</li> <li>We believe that other outbuildings associated with swimming pools, outdoor entertaining areas etc should be allowed in the buffer.</li> <li>The subject land has a buffer indicated along the south and eastern boundaries – if this occurs would Council compensate the owners due to the restrictions that would be placed on the land, 2 acres would be lost.</li> </ul>	Yes	
<ul style="list-style-type: none"> <li>2.0m high wall</li> </ul>	Yes	
<ul style="list-style-type: none"> <li>Who will be contributing to the cost of the wall construction</li> </ul>	Yes	
<ul style="list-style-type: none"> <li>The wall should be a requirement of the only for the B4Z land to ensure no adverse effects on the surrounding properties by way of noise emissions.</li> </ul>	No its not the buffer but the adjoining land use that is the problem. Buffer is required to ensure adjoining land is not blighted	
<ul style="list-style-type: none"> <li>The proposed wall along the southern boundary should not be proposed as the B4 land is mostly developed.</li> </ul>	Comments associated with the above:	
<ul style="list-style-type: none"> <li>How will Council be able to receive contributions from the developed land for construction of the wall given their land is already developed? Colourbond fencing along this boundary is adequate; currently there are no issues with noise emissions.</li> </ul>	The developer of the B4Z land	
Fencing requirements are not determined by Planning Schemes and therefore to enforce such may be difficult as such would be in accordance with the relevant fencing regulations.	Agreed, the trigger to develop wall will be the development of B4Z land, not residential land.	
Gateway Feature – comments relating to:	As the requirement is triggered by development there will be no retrospective requirement on the already developed land.	
<ul style="list-style-type: none"> <li>Please confirm the gateway features are established in the road reserve along Etiwanda Avenue.</li> <li>Please advise who will bear the cost of the features.</li> </ul>	Refer comment above.	
Conclusion – Given the above we believes their needs to be further information provided to determine the issues raised and request we be given the opportunity to make a further submission on the development plan proposed for the subject area.	To be discussed and finalised as part of the development application process.	
	Yes and in other locations as shown on the Vision plan.	
	Refer to DCP#2.	
	Noted for further discussion at detailed design stage.	

<b>S13</b>	<b>RESIDENTIAL DEVELOPMENT PLAN – ETIWANDA AVENUE</b>	
<b>Submitter</b>	Roy Costa on behalf of I & M Dimasi	
<b>Issues</b>	<i>Buffer issues, split zoning of land and access</i>	
<b>Submission summary</b>		<b>Response and Recommendation</b>
Comments on subject land as follows:		
<ul style="list-style-type: none"> <li>The subject land is 7.212ha with road frontage only to Etiwanda Avenue and no access to Benetook.</li> <li>3.746ha is zoned RIZ and the balance id B4Z.</li> <li>Service easements exist at the boundary between the zones.</li> </ul>		This land is outside of the study area.
Development Plan comments as follows:		
<ul style="list-style-type: none"> <li>The buffer area will be through the middle of the property making it difficult to develop the rear portion of the land.</li> </ul>		<ul style="list-style-type: none"> <li>Buffer is required on edge between land uses</li> <li>This is a zoning matter, not within scope of study.</li> </ul>
<ul style="list-style-type: none"> <li>The 2m high wall would land lock the rear of the property – would Council compensate the owners at the value of the land zoning.</li> </ul>		<ul style="list-style-type: none"> <li>B4Z area not within study area</li> </ul>
<ul style="list-style-type: none"> <li>The development plan must provide access to the entire land parcel and as such needs to be determined prior to the development plans being approved.</li> </ul>		Development plan doesn't preclude link, however would not be appropriate to take industrial/commercial traffic through residential area  Detailed design issue
<ul style="list-style-type: none"> <li>If access is provided to the B4Z via Etiwanda Ave than how will this effect the boulevard and associated roads with industrial vehicles required to utilise the proposed roads.</li> </ul>		<ul style="list-style-type: none"> <li>Access will be off Benetook Ave. Link may only be pedestrian and would be subject to a future study which would consider these issues</li> <li>Will need further (+ separate study) before the form and location of this link can be confirmed. Will consider these issues.</li> </ul>
<ul style="list-style-type: none"> <li>When will Council require the wall to be constructed if the agricultural holding continues to operate? How will FMIT &amp; council easements be accommodated.</li> </ul>		<ul style="list-style-type: none"> <li>Wall will be constructed as part of the B4Z development.</li> </ul>
The RDP indicates a possible link between the boulevard and Benetook Ave – this needs to be created and determined immediately to ensure the land can be fully developed.		Noted
Conclusion – Given the above we believes their needs to be further information provided to determine the issues raised as they are important and could jeopardise the future of the property.		Noted for further discussion at detailed design stage.

<b>SI4</b>	<b>RESIDENTIAL DEVELOPMENT PLAN – ETIWANDA AVENUE</b>	
<b>Submitter</b>	Freeman & Freeman on behalf of P & W Kedmenec	
<b>Issues</b>	<i>Location of boulevard and POS</i>	
<b>Submission summary</b>	<b>Response and Recommendation</b>	
Comments on subject land as follows:	Noted	
<ul style="list-style-type: none"> <li>The land parcel consists of all horticultural land fronting Etiwanda on the San Mateo Ave side between the recent residential development and the FMIT depot.</li> </ul>		
Development Plan comments as follows:		
<p>The proposed extension of the Matthews Flinders Drive out to Etiwanda Ave along the common boundary between our clients and the FMIT depot does not seem appropriate or practical in relation to the existing house.</p> <p>The through road would be better suited on the 15th Street side of our client's house.</p> <p>This would allow the proposed SMA and POS area proposed on our clients land to the public open space and drainage areas proposed by the approved subdivision to the north.</p>	<ul style="list-style-type: none"> <li>Alignment puts drainage in public realm and uses landscape amenity, However it is recognised that the final alignment may vary in detailed design.</li> </ul>	
<ul style="list-style-type: none"> <li>We believe RDP should not encourage the creation of roads along common boundaries so avoiding the problem of who constructs the road.</li> </ul>	<ul style="list-style-type: none"> <li>The concept plan applies best practice subdivision principles and seeks to provide an efficient lot layout. In some locations, as here, the layout assumes two adjacent landowners can be developed together because of the efficiencies it allows and the benefits of a co-ordinated approach to design. It is recognised this may not always be possible and if not an alternative layout that retained the underlying structure may be acceptable.</li> <li>The concept plan illustrates how the principles described in the RDP can be applied to the site. It is recognised that the objectives might be achieved with minor variations to the plan and that this would be acceptable, subject to demonstrating design objectives have been met.</li> </ul>	
<ul style="list-style-type: none"> <li>Who will be responsible for the construction of Matthew Flinders Drive, which is 400m long and has different landowners on either side.</li> </ul>	<ul style="list-style-type: none"> <li>The developer has in similar situations paid for the cost. Detailed discussions required at subdivision stage.</li> </ul>	
<ul style="list-style-type: none"> <li>The development plan should state FMIT's intention to remain and generally should be encouraged to relocate their depot in the future.</li> </ul>	<ul style="list-style-type: none"> <li>Noted</li> <li>Check (Sarah?)</li> </ul>	
The 'Stormwater & Wastewater Plan and Vision Plan indicate different requirements with regard to the SMA & POS required	Noted, the plan should be amended.	
<ul style="list-style-type: none"> <li>Concerned by the significant amount of space indicated for public open space but are "comforted" by discussions that they will be appropriately compensated.</li> </ul>	<ul style="list-style-type: none"> <li>Noted.</li> </ul>	
<ul style="list-style-type: none"> <li>Owners are proposing a centrally located POS within the subdivision rather than on the fringe.</li> </ul>	<ul style="list-style-type: none"> <li>Central to broader neighbourhood and open space located to accommodate drainage infrastructure.</li> </ul>	

<ul style="list-style-type: none"> <li>• Believes neighbourhood character has already been set and the proposed wider roads etc would not be appropriate.</li> </ul>	<ul style="list-style-type: none"> <li>• The proposed road standards are required to meet higher standards (new section 56) and the area is large enough to develop its own identity and character.</li> <li>• The proposed concept has been drawn up to ensure a higher standard of amenity and character than exists in the surrounding area,</li> </ul>
<p>Believes the Avenue/Boulevard should be 20 metres and not 25-30 metres and the Standard roads should be 15 and 17m wide rather than 21 metres.</p>	<p>Street character is important to achieve section 56 objectives and other planning and design characteristics. This means creating streets that are not dominated by roads and cars and are attractive, pleasant, safe places to walk. This requires room for landscaping and footpaths and road pavement to generally occupy no more than around a third of the road corridor. It is recognised that these dimensions may change on a site to site basis where appropriate. Consequently it is suggested the plans be amended to show a degree of narrowing possible where road pavement can be reduced.</p>



<b>S15</b>	<b>RESIDENTIAL DEVELOPMENT PLAN – ETIWANDA AVENUE</b>	
<b>Submitter</b>	<b>P &amp; W Kedmenec</b>	
<b>Issues</b>	<b>Location of boulevard and POS</b>	
<b>Submission summary</b>		<b>Response and Recommendation</b>
<p>Development Plan comments as follows:</p> <ul style="list-style-type: none"> <li>No objection to the continuation of Matthew Flinders Drive but do not believe it should be wider than 20metres due to existing neighbourhood character.</li> <li>The proposed extension of the Matthews Flinders Drive out to Etiwanda Ave along the common boundary between our clients and the FMIT depot does not seem appropriate or practical in relation to the existing house. No intention of demolishing house.</li> <li>The through road would be better suited on the 15th Street side of our client's house.</li> <li>Consider adjusting the size of the basins indicated and relocate to forma a village green concept as public open space.</li> <li>Construction cost of Matthew Flinders Drive – who will pay due to Council owning apportion of existing easement and two separate owners either side. One owner has no intention of developing his property in the near future.</li> <li>Believe the road layout and streetscape is best left to the developers who have the opportunity to liase with planners, agents, engineers and consult with Council to best suit individual subdivision requirements.</li> </ul>		<ul style="list-style-type: none"> <li>Refer S14 response</li> <li>Can move alignment where proposed alternative demonstrates connectivity and design standards are met.</li> <li>Refer S14 response.</li> <li>“</li> <li>“</li> <li>“</li> </ul>

<b>SI6</b>	<b>RESIDENTIAL DEVELOPMENT PLAN – IRYMPLE AREA</b>	
<b>Submitter</b>	<b>Lower Murray Water</b>	
<b>Issues</b>	<b>General issues on technical aspects of RDP.</b>	
<b>Submission summary</b>		<b>Response and Recommendation</b>
Context Analysis Plan –		
<ul style="list-style-type: none"> <li>The strip shops indicated do not extend beyond Hassell Street.</li> <li>The oasis Aged Care Home is not indicated.</li> <li>The 8ha subdivision fronting Sandilong is not indicated.</li> <li>A subdivision application for land on the north side of Sandilong next to Irymple Primary School has not been indicated.</li> </ul>		<ul style="list-style-type: none"> <li>Noted, will be amended on plan.</li> <li>Noted, will be amended on plan.</li> <li>“</li> <li>“</li> </ul>
Issues Plan –		
<ul style="list-style-type: none"> <li>Does not show the Francesca Drive subdivision along Karadoc Ave and as an existing subdivision.</li> <li>Does not show the RIZ land next to Irymple Primary School.</li> <li>The rail crossing boom gates have been installed.</li> <li>The strip of shops includes houses.</li> <li>The location of the proposed IGA supermarket is not shown to connect to the note on the margin.</li> </ul>		<ul style="list-style-type: none"> <li>Land is outside study area and on the edge of the plan.</li> <li>Noted, will be amended on plan.</li> <li>Noted, will be amended on plan to show location of IGA.</li> </ul>
Stormwater & Wastewater Management Plan –		
<ul style="list-style-type: none"> <li>The construction of an outfall sewer through the grounds of Irymple Secondary College will be carried out in January 2007, which will provide for the Sandilong residential subdivisions.</li> <li>The Note: Rising Main to be located on public land, should indicate it refers to the sewer rising main required for Irymple Ave sewer pumping stations. This means the area of P.O.S should ‘touch’ the rear of lots in Chandon Court to provide access to a suitable discharge sewer for the rising main.</li> </ul>		<ul style="list-style-type: none"> <li>Noted, will be amended on plan.</li> </ul>
Vision Plan –		
<ul style="list-style-type: none"> <li>The Plan does not provide the POS link– (see above) therefore does not make adequate provision for the sewerage outfall.</li> <li>More expensive sewerage solution would be to direct the rising main out of Irymple Ave and then along Irymple Ave to a maintenance hole at Fifteenth St. The preferred solution would be Chandon Court.</li> </ul>		<ul style="list-style-type: none"> <li>Noted, will be amended on plan.</li> </ul>

<b>S17</b>	<b>RESIDENTIAL DEVELOPMENT PLAN – IRYMPLE</b>	
<b>Submitter</b>	<b>SD &amp; E Pica</b>	
<i>Issues</i>	<i>Supports overall concept generally</i>	
<b>Submission summary</b>	<b>Response/ Recommendation</b>	
Concerns mainly to do with the area between Sandilong and Karadoc  Avenues in 15th Street.	Not in study area	
Current application for bulky goods outlet on the corner of Sandilong and 15th Street despite the VCAT comment on the 'mish mash' of 15th Street.  <ul style="list-style-type: none"> <li>Concerns due to the proximity to schools, safety issues etc.</li> <li>There should be no more bulky goods outlets between Mildura and Irymple if the concept of a welcoming entrance is to occur.</li> </ul>	Forwarded to relevant planner allocated planning permit application for this land.	
The concept of a village green and an upgrade for Irymple is the correct one as this will prevent the infiltration of Mildura into Irymple and provide Irymple with a focus of its own.	Noted	
Wide streets, open space, the identification of entering a residential area and encouraging neighbourhood interaction are all social features which will enhance the area environmentally and socially.	Noted	
The avenue/boulevard perspective will also enhance the entrance to Mildura when travelling along 5th Street.	Noted	
Essential that caveats or covenants are placed on any subdivision defining the type of home to be built, height and construction will either make or break his concept overall.	Noted	
This is an opportunity in all areas in the study to 'get it right' and not build the slums of the future but build sustainable homes in sustainable and user friendly environments.	Noted	

<b>S18</b>	<b>RESIDENTIAL DEVELOPMENT PLAN – IRYMPLE</b>	
<b>Submitter</b>	K. Steinle on behalf of the Horsfall family.	
<b>Issues</b>	<b>Supports overall concept generally</b>	
<b>Submission summary</b>		<b>Response/ Recommendation</b>
Preliminary draft subdivision plan in response to the Irymple RDP.		
We note that the RDP plan as documented has some flexibility in terms of street alignments, types and locations.  Plan therefore provided for discussion purposes only.  In essence the preliminary draft subdivision plan adds weight to and supports the principles outlined within the Irymple RDP.		Noted
Matters that require further attention include:		
<ul style="list-style-type: none"> <li>Possible access point across the north-south reserve as an alternative access to lot 26, in lieu of creating a parallel access road as shown on the plan.</li> </ul>		Requires further investigation during development application stage.
Lots sized are intended to be in the medium size range. With smaller lots to the west of Koorlong Avenue and larger lots to the east.		Noted
Understand that the subdivision and development will be encumbent upon the provision of utility services, including drainage and stormwater head works provision.		Noted
Appreciate the area is identified as Stage 2 and may proceed to development in the 1 to 5 year timeframe.		Comment in relation to stormwater infrastructure provision.

Appendix 4

Revisions Table



